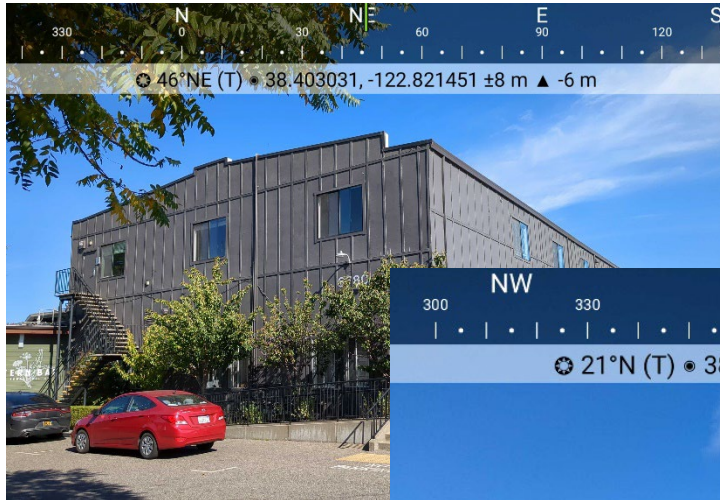


Appendix D  
**Historical Resources Evaluation  
Report**



For CEQA Section 15183, Barlow Hotel Project at 6782 Sebastopol Ave. and Constraints Analysis for 6780 Depot St.), Sebastopol, Sonoma County, California

Yarbrough Architectural Resources  
Edward B. Yarbrough, Proprietor | Architectural Historian  
Saint Helena & San Francisco, California

*On behalf of Aldridge Development, Sebastopol, California*

December 2024

## Executive Summary

Yarbrough Architectural Resources recommends that neither property is individually eligible to the California Register of Historical Resources (CRHR) under any of the CRHR's four significance criteria nor eligible against the National Register of Historic Places (NRHP) criteria. As a result, 6780 Depot Street and 6782 Sebastopol Avenue are not recommended to be historical resources pursuant to CEQA. In January 2024, a draft of this technical study was not scoped to provide impact assessments for a particular project but identified and evaluated the properties according to the significance thresholds established under CEQA by NRHP and CRHR criteria.

In this December 2024 Historical Resources Evaluation Report (HRER or technical study) revision, the Barlow Hotel Project (project) is proposed. The project encompasses the property at 6782 Sebastopol Avenue but does not include 6780 Depot Street in the CEQA Study Area/Area of Potential Effects (APE). Although this technical study continues to provide a recommendation of ineligibility and lack of historical significance for 6780 Depot Street, the HRER now identifies, recommends ineligibility, and therefore, a CEQA finding of *no historical resources impacted* for the property within the APE, namely 6782 Sebastopol Avenue. The APE does not include areas of indirect impact or temporary construction staging areas.

Yarbrough Architectural Resources is not scoped to address the presence or absence of an eligible historic district. Yarbrough Architectural Resources did not receive a response as requested in a November 3, 2023 email to the City of Sebastopol (City) Planning and Community Development Department regarding whether previous evaluations or surveys had led to a determination of an eligible historic district by the City. However, this study found that no listed historic district encompasses the subject properties. Both properties were surveyed in the *Western Sonoma County Historic Resources Survey; Volume 2. Sites Surveyed, Part 1: The City of Sebastopol* (1981, reprinted and revised 2023) and both were excluded from the list of recommended historical resources.

Edward Yarbrough, a qualified architectural historian, conducted a site visit with photographs and notes on November 3, 2023. This Historical Resources Evaluation Report (HRER) was developed based on information obtained from direct observation of site conditions, research at the Sonoma County Records Office, Sebastopol Public Library, Western Sonoma County Historical Society, additional online sources, and other information generally available as of January 2024. The conclusions and recommendations herein are based on information available at the time of the research and site visit. Information obtained from these sources in this timeframe is assumed to be correct and complete. Yarbrough Architectural Resources does not assume any liability for recommendations based upon information that was not obtained or accessible at the time of this technical study. This technical study does not address archaeological resources.

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## Introduction

Yarbrough Architectural Resources prepared this Historical Resource Evaluation Report (HRER) as a technical study on behalf of Aldridge Development (client) in order to inform the client of probable constraints pursuant to the California Environmental Quality Act (CEQA), as amended. The City of Sebastopol is the lead agency for purposes of CEQA and municipal compliance with cultural resources regulations. However, this technical study is provided without a project, permitting request, or other impact considerations regarding the subject properties.

This HRER was commissioned by the client to identify and evaluate historical significance for purposes of identifying potential constraints and to inform development planning for the future. The HRER does not take into account any project, project impacts, or to predict findings that only the CEQA lead-agency may determine. However, this HRER recommends findings regarding eligibility for the buildings and parcels located at 6780 Depot Street and 6782 Sebastopol Avenue, if and when the client may decide to submit a project proposal subject to permitting.

Information obtained from these sources in this timeframe is assumed to be correct and complete. Yarbrough Architectural Resources (YAR) does not assume any liability for findings that a CEQA lead-agency may determine in the future that may differ from the recommendations of this report.

## Project Description

The Barlow Hotel Project (project) consists of a proposed hotel, with up to 83 rooms with additional uses in or on the hotel structure, a parking lot, and ancillary improvements on portions of other parcels. The project is intended to add to the diversity of uses at the existing Barlow market district in downtown Sebastopol and provide support for Barlow industrial producers and retailers as well as surrounding Sebastopol businesses. The project would be approved via a Development Agreement.

The project site includes a hotel site at 6782 Sebastopol Avenue and a separate parking lot site at 385 Morris Street. Collectively, the project site includes the primary hotel, with additional uses in or on the hotel structure as is set forth herein, and parking lot components and related areas for utilities, landscaping, other minor improvements, and construction staging.

The hotel structure would replace the existing 36,402-square-foot Guayakí organic beverage company warehouse building at the Barlow, which spans from Sebastopol Avenue to McKinley Avenue. The parking lot site was formerly a concrete batch plant. Equipment associated with that former use were removed several years ago. Most of the parking lot site is paved. No project development will occur within a 50' setback from the adjacent Laguna de Santa Rosa, in accordance with the project biologist's recommendations and findings. The project will result in 154 new parking stalls on the Batch Plant parcel. Eighteen of the new parking stalls will have EV chargers.

The Barlow is a 12.4-acre pedestrian-oriented development with a wide range of uses, with most square footage occupied by industrial uses, but also including retail shops; artisan restaurants, cafes, and food producers; a community-based supermarket; premium wine, beer, and cider-makers; and other uses. In addition, music and other special events are regularly conducted. The Barlow is a destination for



Sebastopol-area residents as well as visitors to the area. There are approximately 36 tenants in the Barlow, ranging from one tenant occupying less than 230 square feet to a winery occupying over 60,000 square feet. Total building square footage in the Barlow is approximately 222,000 square feet in 18 buildings. Industrial space comprises approximately 61 percent of the Barlow square footage; food and beverage comprise 23 percent; office comprises 7 percent; retail and service uses comprise 5 percent; and vacant, common, and property management comprise approximately 4 percent of the total square footage.

The Barlow is located just east of downtown Sebastopol. To the north are industrial uses along Morris Street. Land uses on the east side of Morris Street include a combination of office, industrial, and utility uses; the Laguna Preserve public park; and the Sebastopol Community Cultural Center. Across Depot Street and across Sebastopol Avenue are a variety of commercial uses, with one retail site having townhomes behind it. Further east on Sebastopol Avenue is a single-family residence, with rental units behind it, as well as the City-owned Park Village mobile home park. Sebastopol Avenue is State Route 12 (SR-12). To the north across Laguna Park Way is the City's Skategarden park and residential uses.

Riparian habitat is present to the north and east of the parking lot site in association with the Laguna de Santa Rosa, which is the largest freshwater wetlands complex on the northern California coast. The Laguna's fourteen-mile channel forms the largest tributary to the Russian River, draining a 254-square-mile watershed which encompasses nearly the entire Santa Rosa Plain. This includes parts of the communities of Windsor, Santa Rosa, Rohnert Park, Cotati, Forestville, and Sebastopol.

## Qualifications of Preparer

Edward Yarbrough, M.S. Historic Preservation, Principal of Yarbrough Architectural Resources is the Principal Investigator/Senior Architectural Historian for the analysis of 511 Logan Street. For over 32-years, Yarbrough developed documentation for projects subject to federal, state, and municipal historic preservation mandates. Yarbrough exceeds the Historic Preservation Professional Qualification Standards for Architectural History, as set forth by U.S. Secretary of the Interior (SOI) and under Code of Federal Regulations, 36 CFR Part 61.

## Regulatory Setting

### State

The State of California implements the National Historic Preservation Act as amended (NHPA) through its statewide comprehensive cultural resource surveys and preservation programs. The California Office of Historic Preservation, as an office of the California Department of Parks and Recreation (DPR), implements the policies of the NHPA on a statewide level. The Office of Historic Preservation also maintains the California Historical Resources Inventory. The SHPO is an appointed official who implements historic preservation programs within the state's jurisdictions.

### California Environmental Quality Act

CEQA, as codified in PRC Sections 21000 *et seq.*, is the principal statute governing the environmental review of projects in the state. CEQA requires lead agencies to determine if a proposed project would have a significant effect on historical resources, including archaeological resources. The CEQA Guidelines

define a historical resource as: (1) a resource in or eligible for listing in the California Register; (2) a resource included in a local register of historical resources, as defined in PRC Section 5020.1(k) or identified as significant in a historical resource survey meeting the requirements of PRC Section 5024.1(g); or (3) any object, building, structure, site, area, place, record, or manuscript that a lead agency determines to be historically significant or significant in the architectural, engineering, scientific, economic, agricultural, educational, social, political, military, or cultural annals of California, provided the lead agency's determination is supported by substantial evidence in light of the whole record.

If a lead agency determines that an archaeological site is a historical resource, the provisions of PRC Section 21084.1 and CEQA Guidelines Section 15064.5 would apply. If an archaeological site does not meet the CEQA Guidelines criteria for a historical resource, then the site may meet the threshold of PRC Section 21083 regarding unique archaeological resources. A unique archaeological resource is "an archaeological artifact, object, or site about which it can be clearly demonstrated that, without merely adding to the current body of knowledge, there is a high probability that it meets any of the following criteria.

- Contains information needed to answer important scientific research questions and that there is a demonstrable public interest in that information.
- Has a special and particular quality such as being the oldest of its type or the best available example of its type.
- Is directly associated with a scientifically recognized important prehistoric or historic event or person" (PRC Section 21083.2 [g]).

The CEQA Guidelines note that if a resource is neither a unique archaeological resource nor a historical resource, the effects of the project on that resource shall not be considered a significant effect on the environment (CEQA Guidelines Section 15064[c][4]).

### California Register of Historical Resources

The California Register is "an authoritative listing and guide to be used by state and local agencies, private groups, and citizens in identifying the existing historical resources of the state and to indicate which resources deserve to be protected, to the extent prudent and feasible, from substantial adverse change" (PRC Section 5024.1[a]). The criteria for eligibility (PRC Section 5024.1[b]) are based on National Register criteria. Certain resources are determined by the statute to be automatically included in the California Register, including California resources formally determined eligible for or listed in the National Register.

To be eligible for the California Register, a historical resource must be significant at the local, state, and/or federal level under one or more of the following criteria:

1. Is associated with events that have made a significant contribution to the broad patterns of California's history and cultural heritage.
2. Is associated with the lives of persons important in our past.
3. Embodies the distinctive characteristics of a type, period, region, or method of construction, or represents the work of an important creative individual, or possesses high artistic values.
4. Has yielded, or may be likely to yield, information important in prehistory or history (PRC Section 5024.1[c]).

For a resource to be eligible for the California Register, it must also retain enough integrity to be recognizable as a historical resource and to continue to convey its significance in the present. A resource that does not retain sufficient integrity to meet the National Register criteria may still be eligible for listing in the California Register, although rarely.

### Historic Integrity

Integrity is the authenticity of a historical resource's physical identity and integrity standards are only applied if the subject property is recommended as eligible following evaluation. Integrity or historical integrity is evidenced by the survival of characteristic features that existed during the resource's period of significance. Historical resources pursuant to CEQA must meet at least one of the CRHR criteria of significance described above and retain enough of their historic character or appearance to be recognizable as historical resources and to convey the reasons for their significance. Historical integrity is considered based on the retention of location, design, setting, materials, workmanship, feeling, and association. These aspects of historic integrity are defined as:

- **Location:** the place where the historic property was constructed or the place where the historic event occurred;
- **Design:** the combination of elements that create the form, plan, space, structure, and style of a property;
- **Setting:** the physical environment of a historic property;
- **Materials:** the physical elements that were combined or deposited during a particular period of time and in a particular pattern or configuration to form a historic property;
- **Workmanship:** the physical evidence of the crafts of a particular culture or people during any given period in history or prehistory;
- **Feeling:** a property's expression of the aesthetic or historic sense of a particular period of time;
- **Association:** the direct link between an important historic event or person and a historic property.

The particular features associated with the eligibility of the resource for listing or eligibility to the CRHR are the resource's most important or character-defining features. Depending on the period of significance, alterations over time to a resource or historic changes in its use may themselves have historical, cultural, or architectural significance. Conversely, a resource that has lost its historic character or appearance may no longer be able to convey its historical significance and would not be recommended as CRHR-eligible nor considered for historic preservation protections under CEQA.

Analysis under the seven aspects of historic integrity is only necessary for a resource that has first been recommended eligible against the California or National registers' criteria of significance. A resource that is not eligible under the criteria does not possess significant aspects of integrity to retain.

## Local

### City of Sebastopol Guidelines for Historic Resources

State law requires the General Plan to include a Conservation Element, which addresses the conservation and utilization of natural resources and the preservation of cultural and historical resources. The protection of historic buildings and historic structures is also addressed in the Community Character Element and in Community Design within the General Plan.

The Conservation of Open Space (COS) policies that address known and potential historical resources are:

- **Policy COS 10-5:** Protect important historic resources and use these resources to promote a sense of place and history in Sebastopol.
- **Policy COS 10-6:** Encourage the voluntary identification, conservation, and re-use of historical structures, properties, and sites with special and recognized historic, architectural, or aesthetic value.
- **Policy COS 10-7:** Encourage historic resources to remain in their original use whenever possible. The adaptive use of historic resources is preferred when the original use can no longer be sustained.

Relevant Community Design (CD) policies are listed under “Goal CD 3: Recognize the Value and Ensure the Preservation of Sebastopol’s Historical and Cultural Resources” as:

- **Policy CD 3-1:** Ensure historic buildings and resources are preserved for future generations.
- **Policy CD 3-2:** Preserve significant historical structures by encouraging adaptive reuse opportunities of historic buildings for contemporary uses.
- **Policy CD 3-3:** Identify and document historical, cultural, and archeological resources including significant sites and structures
- **Policy CD 3-4:** Require new development to avoid the disruption of cultural, archeological, and historical resources to the greatest extent feasible.
- **Policy CD 3-5:** Encourage and support an increased public awareness of local cultural and historical resources.
- **Policy CD 3-6:** Ensure that restoration efforts of City owned historic structures adhere to the original architecture style and period detail of the original structure whenever feasible.

## Sources Consulted

### Western Sonoma County Historical Society

Although access to the physical collections were not available due to limited staffing, the Western Sonoma County Historical Society's online pages on "Local History and Culture" link to diverse holdings maintained by the Sonoma County Library, Special Collections; see below.

### Sanborn Fire Insurance Maps

City of Sebastopol, maps available and consulted included those for 1888, 1891, 1894, 1901, 1903, 1911. No maps were found for the City after 1911; see **Appendix C: Sanborn Fire Insurance and Plat Maps**.

### County of Sonoma

#### Sonoma County Library, Special Collections

In November 3, 2023, Edward Yarbrough reviewed and selected the photographs from the Western Sonoma County Historical Society that are digitally collected and categorized in the Library's Digital Collections online (<https://digital.sonomalibrary.org/explore/collections/list/digital-collection-name/55588>). The HRER's analysis of changes through time from street and aerial views and in selected figures, as credited in captions, benefit from the photographic collection. In addition, the Library's Sebastopol branch's local history collection provided some books referenced herein.

The *Final Report of the 1981 Western Sonoma County Historic Resources Survey* (reprinted 2023) provided comparative evidence for the identification and evaluation of the subject properties relative to previously identified ineligible, eligible, and known historical resources in and around the City of Sebastopol.

#### Assessor's Office

On November 3, 2023, Edward Yarbrough researched the *Index to Grantor and Grantee Deeds* books held at the Assessor's Office in Santa Rosa; see **Appendix B: Property and Ownership Records**.

#### Arch GIS Maps

Zoning and Land Use map with data layers; see **Appendix C: Sanborn Fire Insurance and Plat Maps**.

### United States Geological Survey

Topographic map collection including the City of Sebastopol for 1935, 1942 & 1954 ; see **Appendix C: Sanborn Fire Insurance and Plat Maps**.

### California Historical Resources Northwest Information Center

The following sources of information were consulted online:

Built Environment Resources Directory (BERD) for Sonoma County, which includes:

- National Register of Historic Places (NRHP)
- California Historical Landmarks

- Office of Historic Preservation Historic Properties Directory

California Inventory of Historical Resources

Special Research Collections at the UCSC and UCSB Libraries (aerial images and historic maps)

### **Built Environment Resource Directory (BERD)**

The Built Environment Resource Directory shows that the subject properties were not in the Sonoma County list of historical resources.

## Natural Environment

The subject properties are located to the west of an expansive wetlands. These wetlands, the Laguna de Santa Rosa, are an integral part of the Sebastopol region's fertile loamy soils, the foundation of the agricultural success of the area, and created broad level plains, an ideal topography for railroad and warehouse development.

The Laguna de Santa Rosa is one of the largest freshwater wetlands complexes in the northern California coastal mountain region. Hosting extraordinary biological diversity, the Laguna's 22-mile-long channel forms the basin of the largest tributary to the Russian River. The Laguna drains a 254-square-mile watershed that includes most of the Santa Rosa Plain. The Laguna includes portions of Windsor, Santa Rosa, Rohnert Park, Cotati, Forestville, and Sebastopol (SonomaCounty.com 2023).

The Laguna de Santa Rosa is a vast verdant basin that absorbs excess water during heavy precipitation events. Thereby, the Laguna reduces erosive damage through containment and naturally slowing drainage. Warehouses built atop raised concrete footings for railroad freight car loading and level ground for laying railroad tracks reflect the economic and topographic suitability of the level properties. The natural environment is integral to the location and structural forms of the subject properties. See the Property History subsection of the Historic Context for figures and further discussion of the subject properties.

## Historic Context

### Coast Miwok and Southern Pomo Indians

The Federated Indians of Graton Rancheria provide their own history on their website. This subsection of the Historic Context relies on this history to provide a context for the continuity of Coast Miwok and Southern Pomo people before and during the establishment of Sebastopol. Note, this Historical Resource Evaluation Report does not address subsurface or archaeological resources or sensitivities.

Prior to European contact, the residents of modern-day Marin and Sonoma Counties lived in bands belonging to two linguistic and cultural groups: the Coast Miwok and Southern Pomo. The cultural groups lived in close proximity to each other (Federated Indians of Graton Rancheria 2023).

According to the Federated Indians of Graton Rancheria, their community is a "federation of Coast Miwok and Southern Pomo groups recognized as a tribe by the U.S. Congress. The Miwok of west Marin County have, through the years, been referred to as Marshall Indians, Marin Miwok, Tomales, Tomales Bay, and Hookooeko. The Bodega Miwok (aka, Olamentko) traditionally lived in the area of Bodega Bay. The neighboring Southern Pomo Sebastopol group lived just north and east of the Miwok. The town of Sebastopol is located about one mile midway between the north boundary of Miwok territory and the southern edge of Southern Pomo territory." See **Figure 1**. (Federated Indians of Graton Rancheria 2023).

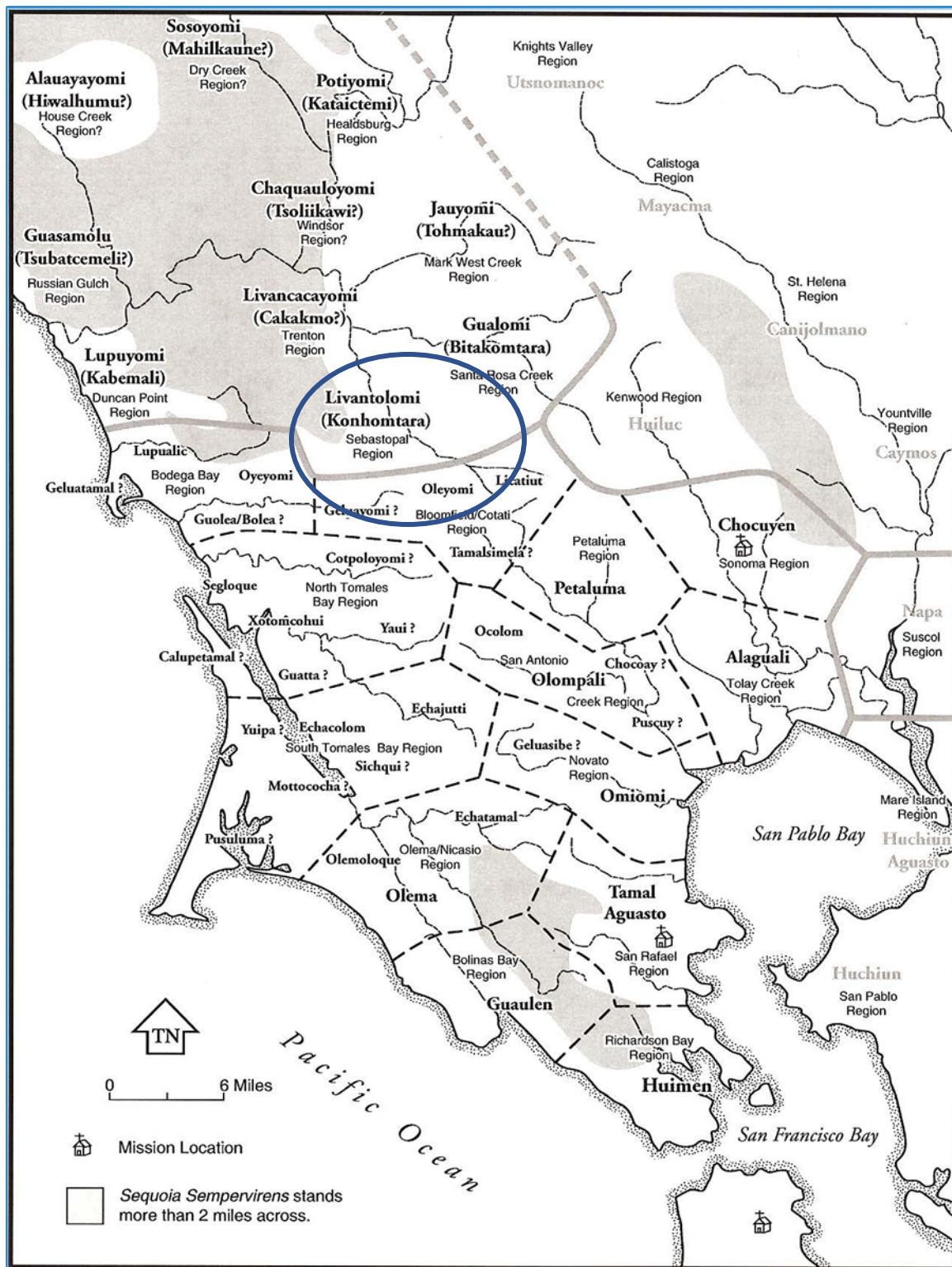


Figure 1 Randall Milliken's map of Coast Miwok and Pomo communities with approximate locations and boundaries during the Mission Period, discussed in the next section. Note that the Sebastopol region is set at the southern end of Pomo territory by the Livantolomi (Konhomtara) and near the northern Coast Miwok territory by the Oleyomi settlements; see the author's oval outlined boundary. (Milliken 2009)



## Contact-Era Ethnography

The year 1579 was the earliest recorded account made by the Europeans of the Coast Miwok people on the coast of modern-day Marin County in the Point Reyes area, as documented in a diary of Francis Fletcher. Fletcher was chaplain aboard Sir Francis Drake's ship, the *Golden Hind*; see **Figure 2**. Soon after in 1595, Coast Miwok bands encountered the crew of the *San Agustin*, a Manila Galleon, captained by Sebastião Rodrigues Soromenho with a crew of Filipino mariners. During the Mission Period of 1779–1823, Mission San Francisco de Asís (i.e., San Francisco's Mission Dolores), Mission San Rafael Arcángel in San Rafael, and Mission San Francisco Solano in Sonoma used Indians, including the Coast Miwok and Southern Pomo people, as forced laborers. (Fletcher 1637; Federated Indians of Graton Rancheria 2023).



*Figure 2 A 1590 engraving by Theodor de Bry of Sir Francis Drake and his company interacting in 1579 with coastal-California Native Americans, likely the Coast Miwok (Fletcher 1637).*

As early as 1830, Domingo Felix, a former Filipino mariner, married a Coast Miwok woman named Euphrasia Valencia. In 1861, Domingo and Euphrasia started a family who would later settle Lairds Landing. Until 1955, descendants of the couple remained at Lairds Landing and, according to the Federated Indians of Graton Rancheria, many Coast Miwok trace their lineage to this couple (Federated Indians of Graton Rancheria 2023).

The territorial lands of the Southern Pomo are in Sonoma County, south of the Russian River. The Southern Pomo were the first known inhabitants of what is now the City of Sebastopol with villages

adjacent to the resource-rich Laguna de Santa Rosa. Renowned California anthropologist Alfred L. Kroeber stated:

*Batiklechawi, at Sebastopol at the head of the slough known as Laguna de Santa Rosa, was an important town, and therefore presumably the headquarters of a division [of the Southern Pomo]. Another group tentatively may be inferred as having occupied the bulk of the shores of the laguna (Federated Indians of Graton Rancheria 2023).*

The Federated Indians of Graton Rancheria state in their history that most of the Coast Miwok continued to live in their traditional lands through the 20th century, working in sawmills, as agricultural laborers, and fishing to supplement their incomes.

The first Indian rancheria under U.S. hegemony was established in 1920 for Coast Miwok, Southern Pomo, and other Indians living in the region. However, when Indians began to settle the 15-acre rancheria land, they discovered that all but three-acres were inhospitable and set within the Laguna de Santa Rosa basin. In 1958, the federal government terminated the trust agreement with the rancheria with only an acre of Miwok-descendant Glora Armstrong's private land remaining in Native American possession. After an eight-year fight, the Graton Rancheria Restoration Act was passed by the U.S. Congress on December 27, 2000 and in 2008 the Federated Indians of Graton Rancheria acquired 254-acres under a *Final Agency Determination to Take Land into Trust* under 25 Code of Federal Regulations (CFR) 151 through the Department of Interior, Bureau of Indian Affairs (Federated Indians of Graton Rancheria 2023)

### Colonial Europeans and the Mexican Period

The Spanish and Russian voyagers provided additional information about encounters with the Coast Miwok and their occupancy of the area, proving these Indian peoples continued to live in this area over the ensuing centuries. Russian outposts were established at Bodega Bay and Fort Ross in 1809 and 1812, respectively.

The Spanish missions and the Mexican occupancy impacted this area of California. Mission San Francisco de Asisi (Mission Dolores), Mission San Rafael Archangel and Mission San Francisco Solano used Indians, including the Coast Miwok and Southern Pomo people, as their labor source. Records from these Missions are still used to substantiate the Native culture and continuous occupation and for genealogical research.

From its early beginnings, European encroachment in central and northern California focused heavily around the San Francisco Bay as a safe landing and central transportation hub. The Bay Miwok and Pomo to the north of the Golden Gate, and Ohlone to the south, were all particularly impacted by Spanish missionization. Milliken's (2010) *Contact-Period Native California Community Distribution Model* reconstructs California Indian community ethnogeography at the time of Spanish settlement (Yarbrough Architectural Resources 2023).

The first large groups of Coast Miwoks that went to Mission of our Seraphic Father San Francisco de Asis (Mission Dolores) were part of a mass migration occurring in 1794–1795. Then, between 1800 and 1817, Coast Miwok dominated the incoming Mission Dolores population, at which time Mission San Rafael was founded, in late 1817, and began affecting Tribes throughout the north bay. Mission records show the

incorporation of neophytes from the southern missions, ongoing baptism of northern Coast Miwok populations, and large group baptisms of Pomo speakers beginning in 1822 (Yarbrough Architectural Resources 2023).

When missions were secularized in the 1830s, many Natives transitioned from forced labor for the Catholic Church to servitude to Mexican landowners. Others were left to find their way home, often finding their childhood homes and families gone; many others never returned. Settlements gathered at Olompali, the site of a large Coast Miwok village with an occupation extending into precontact times, and at Nicasio, where the San Rafael Christian Indians were granted 20 leagues (80,000 acres) of mission lands in 1835 (Federated Indians of Graton Rancheria 2023).

## American Period

### From the Mexican-American War to Sebastopol's Founding

In 1848, after a brief conflict, Mexico ceded California to the United States. An American military map of 1847 (see **Figure 3**) shows "Upper and Lower California" and how few troops were required to wrest control of Mexico's "Military Stations." Unrest and coups in both Mexico City and in the Mexican state of Alta California prevented the Republic of Mexico from ever exacting military and governmental control of much of the remote state.

With the discovery of gold that same year and the subsequent gold rush of 1849 into the early 1850s, the population of California grew exponentially. Rancho owners, often rich in land but with little cash, became indebted to American bankers and lawyers, who routinely took title of rancho lands in exchange for legal representation of the Mexican landowners who had to prove their property ownership. With U.S. control of California came surveyors and for the first time reasonably accurate maps of the region were produced (Yarbrough Architectural Resources 2023).

When the first California legislature created Sonoma County in 1850, which then also included most of what is now Mendocino County, the new county government found nothing but confusion where rancho ownership and property boundaries were concerned. Few of the rancheros lived there and the true acreage of their properties was unknown. Overlapping claims, ambiguities in boundaries, and illegal transactions exacerbated the situation (CSU Monterey Bay 2017; Yarbrough Architectural Resources 2023).

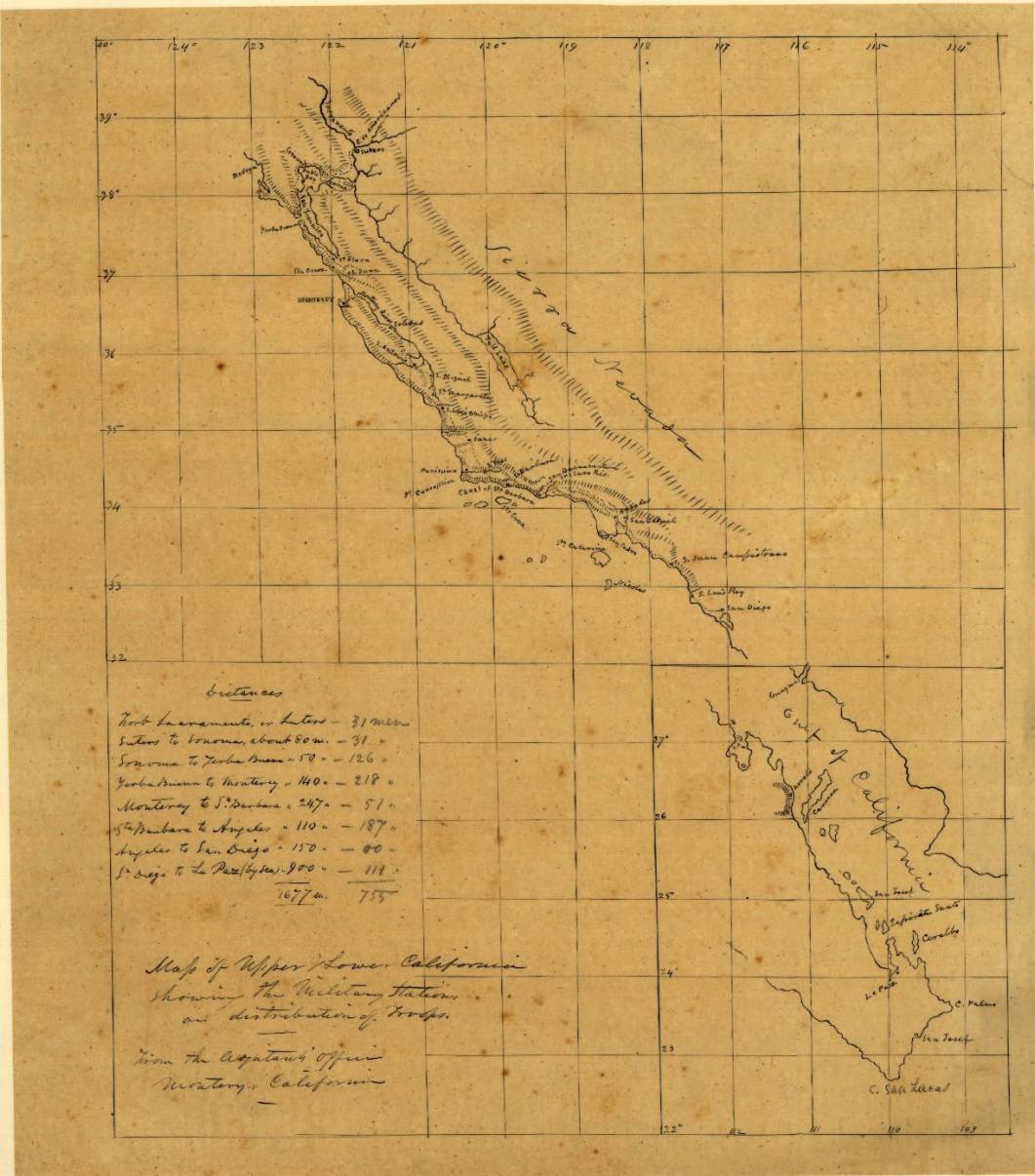


Figure 3 Map of 1847 showing American soldiers stationed in every major California settlement. Notations provide distances between settlements and reflect California's sparse population during the Mexican period, only 31 American soldiers required to hold Sacramento and for Sonoma and the most populace areas of San Francisco to Monterey and Santa Barbara to Los Angeles held by 218 and 187 soldiers, respectively (CSU Monterey Bay 2017).

The U.S. Congress passed legislation in 1851 determined to "ascertain and settle" land claims in California and created a three-man Board of Land Commissioners, appointed by the president, to examine and determine the validity of the Spanish and Mexican land grants in California. The land commission, which began hearings on January 2, 1852, represented only a first legal step, as both sides--the land claimant and the United States--had the right of appeal in the California district courts, and when necessary, in the state supreme court. As common practice, the U.S. attorneys entered an appeal to the courts, extending the litigation and making the average length of time between initial petition to the commission and final patent on the land seventeen years (Yarbrough Architectural Resources 2023).

In the midst of this lengthy legal process, most claimants went bankrupt. Some who had received confirmation of their grants from the land commissioners had their titles invalidated in district courts. Presented with financial difficulties and the pressing demand for land from growing numbers of Americans in California, some sold sections of their grants before receiving a final American land title and patent. Consequently, title to lands remained obscure for years after California became a state (Yarbrough Architectural Resources 2023).

With some 1400 land claims before the Board of Land Commissioners, lawyers were in constant demand in California. San Francisco's bar included some of the most skilled and knowledgeable attorneys in the state. Many of these lawyers directed their energies toward acquiring property themselves, often accepting rancho lands as payment for their services. By the close of 1866, vast tracts in Sonoma County had fallen into the hands of San Francisco attorneys, while not one of the original rancho grantees remained to witness the nearly completed American takeover of the land (Yarbrough Architectural Resources 2023).

### Sebastopol

This history of the City of Sebastopol is largely based on the research and scholarship of Frank Baumgardner, Evelyn McClure, Stacy Rappurt, Rae Swanson, Margaret Marshall, Sally Morrison Giberti, Claire Clarke, Will Roberts, and Betty Short of the Western Sonoma County Historical Society and the Society's Archives. Their collective efforts produced *Sebastopol* (Western Sonoma County Historical Society 2003) and *Sebastopol's Gravenstein Apple Industry* (Western Sonoma County Historical Society 2011).

Sebastopol began as a permanent settlement in the 1850s with a post office and small trade center for the few farmers in the surrounding rich agricultural region. As California's population swelled after the westward migration and the Gold Rush of the 1850s, more and more settlers drifted into the fertile California valleys north of San Francisco to try their hand at farming (Hansen et al. 1962; Western Sonoma County Historical Society 2003).

The name of Sebastopol first came into use in the late 1850s because of a prolonged and lively fist fight in the newly formed town which was likened to the long British siege of the Russian seaport of Sebastopol during the then raging Crimean War. Evidently, many Americans in the west sympathized more for the Russian than the British cause as there were at one time four other Sebastopols in California; one in Napa, renamed Yountville, and one each in Tulare, Sacramento and Nevada counties (all since abandoned). The name hung on in Sonoma County and was officially adopted in the 1860s (Western Sonoma County Historical Society 2003).

An outlying settlement, the Sebastopol area grew slowly until the 1890s, when the railroad connected Sebastopol to Santa Rosa, Petaluma and the world. The railroad facilitated the development of the fruit industry with fast and dependable shipment of San Francisco and the east coast. The apple industry took prominence with berries and hops not far behind and Sebastopol entered a period of significant growth. Because the area was agricultural, most of the population was scattered in the region surrounding the town, so that the community proper remained small. In 1911, Sebastopol had a population of 2,000 compared to Santa Rosa's 12,000 and 10,000 people in Petaluma. With the railroad inspired prosperity,

a self-sufficient business community developed and became the foundation for the present downtown (Western Sonoma County Historical Society 2003; Western Sonoma County Historical Society 2011).

Early in Sebastopol's history, however, settlers had to travel dusty and rutted roads to Santa Rosa and Petaluma for the many goods and services not offered either at local stores or by traveling peddlers. In the 1850s, residents in the area applauded the opening of Miller & Walker's general merchandise store where they could conveniently obtain some of the materials needed for planting, making clothing and preserving food. Joseph Miller and John Walker settled near the home of Joaquin Carrillo which reportedly stood facing east about where Petaluma Avenue and Santa Rosa Avenue meet today. They joined in partnership and opened a small general merchandise store and post office for the north coast area, with Mr. Miller as postmaster and J.H.P. Morris as clerk. The Miller & Walker store and post office, then called Bodega because it was in the large rancho Bodega, was also a stopover for the stages from Gualala and Petaluma. In 1852, mail was carried once a week from Benicia to Napa, to Sonoma, Santa Rosa, Miller & Walker's store and then to San Rafael. Thus, commerce began at the crossroads (Western Sonoma County Historical Society 2003).

A few years later, the clerk, Mr. Morris, bought land a short distance north of Miller & Walker's store and set up his own establishment. Morris' combined store and saloon was housed in a building which he bought from Miller & Walker and had drawn by oxen to the new location. Milled lumber was a prized commodity with only one mill operating in the Sebastopol vicinity at the time. Mr. Morris named the new settlement Pine Grove, after what he mistakenly thought was a grove of pine trees, rather than sequoia. Morris laid out town lots and offered to deed a lot to anyone who would start a business (Western Sonoma County Historical Society 2003).

Sebastopol's first crop was potatoes, introduced by W. McReynolds in the 1850s. The potatoes were transported to Petaluma, stored in warehouses, and then transferred to boats that continued the trip down the Petaluma River, eventually ending up in San Francisco where markets clamored for fresh vegetables and game to supply a bustling population. Over-production soon made the price of potatoes drop and production switched to other crops.

With the arrival of the Northwestern Pacific Railroad to Santa Rosa in 1871, and then to Sebastopol twenty years later, the farmers were put in touch with distant as well as local markets for their exceptional produce (See **Figure 3**). By 1904, a local electric line was laid from Petaluma to Sebastopol, Forestville, and Santa Rosa. Nicknamed the "Cow's and Chicken's Line", because of its local freight and frequent stops, the Petaluma and Santa Rosa Electric Railway provided local farmers with a direct link to the main freight routes in Santa Rosa and Petaluma as well as furnishing a much-needed suburban passenger service (See **Figure 4**). The Sebastopol electric line depot still stands on South Main Street (Western Sonoma County Historical Society 2003; Western Sonoma County Historic Resource Survey, Volume 1 1981/2023).



*Figure 4 1905 depot on Depot Street, west of 6780 Depot Street (Courtesy of the online photographic archives collections of the Western Sonoma County Historical Society, accessed via the Sonoma County Library).*



*Figure 5 1917 photograph of the railroad station designed by Petaluma architect Brainerd Jones that remains in its original location (Courtesy of the online photographic archives collections of the Western Sonoma County Historical Society, accessed via the Sonoma County Library).*

Farmers discovered rapidly that the land between the Laguna, a marshy creek running north in the Santa Rosa Valley, and Green Valley Creek, two miles west of Sebastopol, was an especially fertile region. The sandy loam soil was well suited to the growing of fruit, hops and garden products as irrigation was virtually unnecessary. Eventually the area, including Sebastopol, became known as the Gold Ridge District for the rich returns of fruit produced (Western Sonoma County Historical Society 2011).





*Figure 6 Photograph from 1976 in Sebastopol shows apple trees in bloom in an orchard with redwood and other conifers as a backdrop (Courtesy of the online photographic archives collections of the Western Sonoma County Historical Society, accessed via the Sonoma County Library).*

The Russians were the first to have Gravenstein apples growing in the north coast region near Bodega in the early 1800s. Gravenstein apples were presumably named after Slot (or Gravenstein Castle after the German name) on the north shore of Flerizborf Fjord on the Jutland Peninsula of Denmark. In the 1880s berries, grapes, cherries, hops, and apples were equally among the major crops grown in the Sebastopol region. However, the Gravenstein apple promptly outstretched the others and became Sebastopol's most distinctive product bringing widespread fame to the community (See **Figure 5**), (Western Sonoma County Historical Society 2011).

Nathaniel Griffith planted the first commercial orchard of Gravensteins in the area with Luther Burbank assisting with the propagation. Luther Burbank, famed for his horticultural ingenuity, had his 18 acres of experimental gardens located close to the town limits. Griffith's success with Gravenstein orchards stimulated others and by the 1890s apple orchards were spread everywhere throughout the Gold Ridge District. Packing plants, canneries and apple dryers came soon after (Western Sonoma County Historical Society 2011).

In its early stages, the apple industry fresh-packed and shipped the early ripening Gravensteins to the east coast and sometimes foreign markets before other varieties were ripe and market ready. When processing techniques were developed, canneries and dryers produced dried apples, apple juice, applesauce, and vinegar as they do today. Some of the old style of apple dryers can still be seen in the region surrounding the town (Western Sonoma County Historical Society 2011).

As the agricultural industry in Sebastopol expanded, workers, often immigrants, arrived to help with the harvesting. They brought with them varying cultural backgrounds that shaped the character of the growing settlement. The apple industry brought a steady rural prosperity to the town and Sebastopol was incorporated in 1902 with schools, churches, hotels, canneries, mills, wineries, and an opera house. The 1906 earthquake reduced most of the earliest buildings to rubble, as elsewhere in Sonoma County. The town was rebuilt anew. The rise of the automobile and truck dramatically affected both agricultural shipping methods and the main street environment with Sebastopol Avenue becoming a well-traveled highway (Western Sonoma County Historical Society 2011).

The vernacular architecture of farmers, farm workers, and tradesmen reflected functional forms and readily available building resources, such as the harvesting and milling of vast stands of old-growth redwoods from the coastal mountains ranges immediately to the west of Sebastopol. The Gothic Revivals, Greek Revivals, Queen Anne, Colonial Revival, Mission Revivals, bungalows, and international styles of the 20th century are represented in Sebastopol's residential and commercial architectural. However, particularly in eastern Sebastopol, utilitarian buildings to process, store, and ship agricultural products sprung up, particularly in the first quarter of the 20<sup>th</sup> Century (Western Sonoma County Historic Resource Survey, Volume 1 1981/2023).

## Property History

### *Summary of Property Identification*

Based on information shown below, 6780 Depot Street was built between 1903 and 1911 as the Enterprise Bottling Company. Research for this report was not able to identify an architect or builder from public records examined. The warehouse at 6782 Sebastopol Avenue was built circa 1924/1925 by the Sebastopol Cooperative Canning Company. No records of the architect or builder were identified.

### *Evidence of Property History*

The land possessed agricultural advantages and, later, suitability for light industrial uses: rich silt and loam soils to support the mineral and organic needs of orchards and other crops, such as at John and Barbara Brown's farm that preceded construction of the railroad warehouses (see **figures 7 & 8**); a level surface for agricultural access roads and later for railroad tracks; and, access at the intersection of downtown, once vast orchards, and proximity to larger markets and transportation networks to the east of Sebastopol (see **figures 9 & 10**).

At the time of the 1911 Sanborn Insurance Map both sides of the block where 6780 Depot Street sits included 5-residences, a Roman Catholic Church, and the H. H. Laton finished lumber warehouse and lumber yard.

The fruit processing and canning buildings and railroad spurs that connected them to distant markets were built west of the Laguna de Santa Rosa wetlands between 1903 and 1911, according to Sanborn

Fire Insurance maps from those dates; see **Appendix C: Sanborn Fire Insurance and Plat Maps and Aerial Photographs**. The first railroad warehouses recorded in the area by 1911 were the Italian-Swiss Colony Winery & Distillery, the Central California Canneries Company, and a box-making and warehouse that may have been the Enterprise Bottling Works at 6780 Depot Street. These structures were connected to railroad track spurs from the Western Pacific Railroad line, a company formed in 1903 to compete with the Southern Pacific Railroad monopoly (Western Sonoma County Historical Society 2003).

6780 Depot Street primary unit, now the 2-story façade with front porch, is labeled as a space for “box making” and had a partial deck along the eastern portion of the façade. The front portion is labeled as having wood or post foundations while the rear extension that runs back to McKinley Street had a concrete perimeter wall.

In 1911, the property that would become 6782 Sebastopol Avenue (the avenue also previously referred to as both Santa Rosa Avenue and Santa Rosa Road) was bisected by Depot Street. No building appears at 6782 Sebastopol Avenue on that date.

On page A-1 of the 1918 Grantee/Grantor Book, Sonoma County Assessor’s Official Records, Nulaid Farmers Association sold the parcel at what would become 6782 Sebastopol Avenue to the Sebastopol Cooperative Cannery on August 28, 1918. The extant building on the parcel had not yet been constructed. On September 18, 1924, a short article in the Santa Rosa Press Democrat (see **Figure 11**) announced that the Sebastopol Coop Cannery Company was organized with 4,000-shares by “Newton B. Kinley of Santa Rosa, I. N. Cable, W. O. Barnes, James Blundin, Henry Elphick, Sebastopol; Fred K. Hammell, Petaluma, and Wendell Henderson, Kelseyville.” Further, the article announces, “the company plans to erect a modern cannery for the preserving of fruits, vegetable and berries grown in Sonoma County.” With this evidence, the date of construction for the Sebastopol Coop Cannery building at 6782 Sebastopol Avenue was likely begun in 1924 and completed in 1925.

The initial investors listed in the newspaper article (see **Figure 11**) did not appear in further research to be historically significant individuals. The four investors hailing from Sebastopol, namely Cable, Barnes, Blundin, and Elphick, did not appear in other newspaper searches nor in Western Sonoma County Historical Society resources online.



*Figure 7 In the late-19<sup>th</sup> and early 20<sup>th</sup> centuries, the western portion of John & Barbara Brown's farm property may have included what would later be partitioned as the Birdie Miller Cnopius tract, including the later parcels of the subject properties' parcels (Courtesy of the online photographic archives collections of the Western Sonoma County Historical Society, accessed via the Sonoma County Library)*



*Figure 8 Mr. John A. Brown and his wife Barbara Brown outside their farm house at the east end of Sebastopol. The house is now the Animal Kingdom Veterinary Hospital at 6742 Sebastopol Avenue (Courtesy of the online photographic archives collections of the Western Sonoma County Historical Society, accessed via the Sonoma County Library).*



*Figure 9 1944, September 3 view of the Sebastopol Coop Cannery industrial canning and processing plant located at Highway 116 and Occidental Road. This facility shares the level ground of the subject properties but in this case 2.6-miles north. The Sebastopol Coop Cannery Company's primary operation was also constructed on level valley-floor ground (Courtesy of the online photographic archives collections of the Western Sonoma County Historical Society, accessed via the Sonoma County Library)*



*Figure 10 1951 at McKinley Street, which runs east-west behind both industrial structures and through the middle of the site. The building with light cladding and gable roof to the left of the cars is 6782 Sebastopol Avenue and the darker building with a hip roof and a conveyor bridge over the street is the real portion of 6780 Depot Street (Courtesy of the online photographic archives collections of the Western Sonoma County Historical Society, accessed via the Sonoma County Library).*

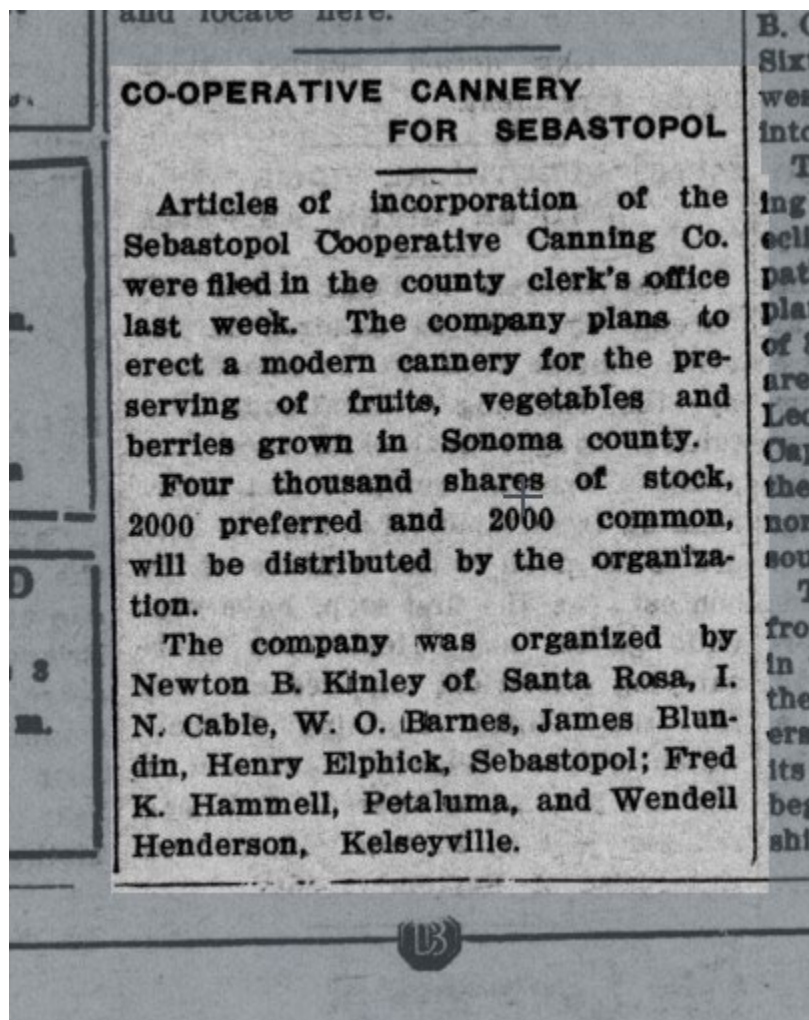


Figure 11 "Plans to erect a modern cannery for the preserving of fruits, vegetables and berries grown in Sonoma County" by the newly organized Sebastopol Cooperative Cannery Company were announced on September 18, 1924 in the Santa Rosa Press Democrat newspaper.

In **Figure 12**, in the center of the 1965 aerial photograph, 6780 Depot Street is the building directly above the elongated form of 6782 Sebastopol Avenue. Note that in both this aerial photograph and the view of McKinley Street in **Figure 10**, that an overhead bridge is visible. This may have been a part of an overhead conveyer belt system. Its presence and a shared address number suggests that 6780 Depot Street was historically connected to operations across the street and to its rear at 6780 McKinley Street, a property clearly labeled Enterprise Bottling Works in the 1911 Sanborn Fire Insurance Map.

Railroad spurs and access to main lines were increasingly abandoned in the County as products were shipped by truck, a trend that accelerated following World War II. However, railroad transport continued to be a primary method of shipping product from the Sebastopol Coop Cannery's warehouse. For example, the Sebastopol Coop Cannery shipped four entire railroad freight cars of Apple Time juices and sauces to market weekly, according to the Santa Rosa Press Democrat newspaper in 1968 (See **Figure 13**)



*Figure 12 Aerial photo taken from the east of the subject properties in 1965 (Courtesy of the online photographic archives collections of the Western Sonoma County Historical Society, accessed via the Sonoma County Library).*



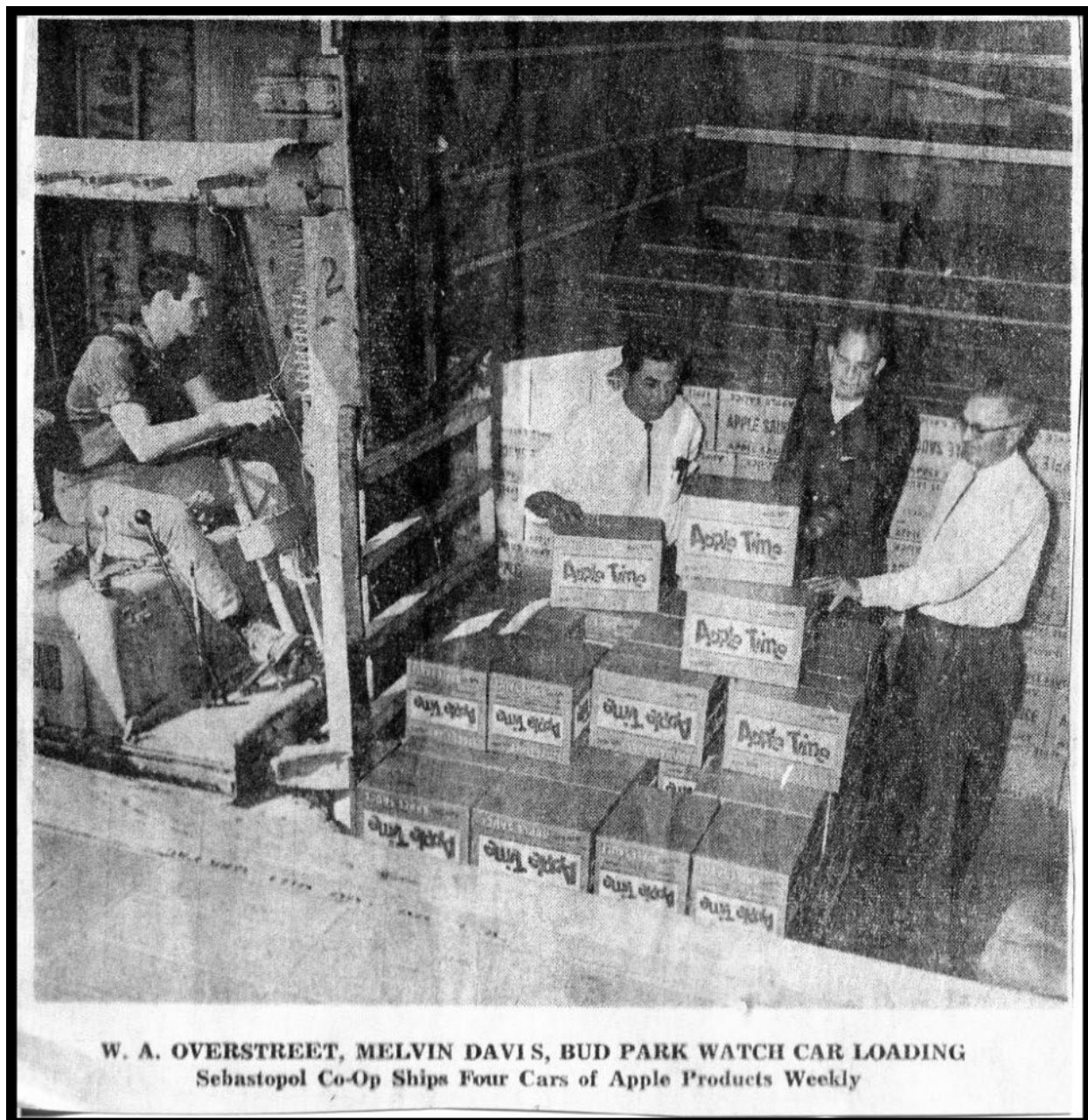


Figure 13 From the Santa Rosa Press Democrat, a forklift driver and three managers of the Sebastopol Cooperative Canning Company pose with "Apple Time" products that are boxed and ready to load on freight rail cars, circa 1968 clipping (Courtesy of the Western Sonoma County Historical Society)



*Figure 14 This 1970 photograph shows that original, circa 1924, fenestration and siding of the façade and side elevations of 6782 Sebastopol Avenue had been replaced with manufactured wood siding, known as T1-11 and fixed and sliding aluminum frame windows. Note that a contemporary sign of backlit plastic sign above the entrance reads “Sebastopol Co-op Cannery.” (Courtesy of the online photographic archives collections of the Western Sonoma County Historical Society, accessed via the Sonoma County Library).*

Both warehouses at 6780 Depot Street and 6782 Sebastopol Avenue evidence significant changes to fenestration, siding, and use in the later half of the 20<sup>th</sup>-Century to the present. These alterations are commonly reflected in “Year Built” dates on parcel reports. 6780 Depot Street’s Parcel Report, Parcel #004-750-034-000 provides a Year Built of 1965. 6782 Sebastopol Avenue’s Parcel Report, Parcel #004-750-030-000 gives a Year Built of 1953. The aluminum-frame windows and manufactured siding seen on 6782 Sebastopol Avenue in **Figure 14** may reflect 1953 alterations. 6780 Depot Street shows many alterations to the primary mass facing Depot Street, including late 20<sup>th</sup> or even early 21<sup>st</sup>-Century bronzed aluminum sliding tinted windows and exterior mounted concrete and steel stairways with switchback landings, and a shed-roof porch with side wheelchair ramp on contemporary concrete footings.

See **Appendix B: Property and Ownership Records** and **Appendix C: Sanborn Fire Insurance and Plat Maps and Aerial Photographs** for additional primary evidence and visual information.

### History of a Warehouse Construction Technology: Reinforced Concrete

The development and methodological diffusion of reinforced concrete technology was well established by the time railroad warehouses were constructed in Sebastopol. This technology was essential to support the weight of agricultural products stored for transportation. Concrete without the tensile strength of iron or steel fails under unsupported pressure from weight on beams and floor-slabs, such as those used in the subject warehouses. Reinforced concrete is ideally suited for cantilevered strength, such as when heavy equipment and boxes of products require reliable support on a warehouse floor.

Although reinforced concrete was first proposed by French architects in the late-18<sup>th</sup> Century, its widespread use began with pioneers in England in the 1850s and then in the 1860s in the United States and in California in particular. From Ipswich, England, Ernest L. Ransome immigrated to the San Francisco Bay Area, where he opened a concrete block manufactory in 1868. Far from the iron supplies of the eastern United States, cast iron facades were expensive and lacked the strength and earthquake and fire resilience of reinforced concrete, which only required enough iron for reinforcing bar. His initial projects from 1883 to 1889 in reinforced concrete were for a sidewalk in Stockton and floor slabs for a flour mill on the East Bay shore, a winery in Saint Helena, and an industrial warehouse in Alameda (Condit 1982).

Ransome and those that followed advanced a technology that made railroad warehouses, like the subject properties, capable of supporting greater weight and larger volumes of storage in parallel to the advancements of mass product transportation through freight rail. Although not early examples, which would include industrial and storage buildings in California beginning in the 1880s, the subject warehouses from the first half of the 20<sup>th</sup>-Century reflect the practicality of reinforced concrete in warehouse foundations. The wood frame and truss construction of the subject warehouses, where visible, are ubiquitous and, like contemporary spans, limited the width of the elongated rail-side buildings (Condit 1982).

## Physical Description

The subject properties are adjacent to one another and sit astride a former railroad track spur in a district of fruit and vegetable processing and storage facilities. The subject properties are part of The Barlow, a 12-acre outdoor market district featuring local food, wine, beer, spirits and crafts made onsite by Sonoma County artisans.

The parcels are described below, followed by a record of the buildings on the parcels in their current condition. See **Appendix C: Sanborn Fire Insurance and Plat Maps and Aerial Photographs** for the County Assessor's Parcel Map of the August 28, 1918 Partition of Birdie Miller Cnopius, Assessor's Map Book 4, Page 75.

### Parcels

The two parcels are identified by Assessor's Parcel Numbers (APN) and references as follows:

- 6782 Sebastopol Avenue-
  - APN: 004-750-030-000
  - Partition of Birdie Miller Cnopius
  - Recorded May 7, 1918 in Book 35, Maps pages 22-00
  - Lot 30
  - 1.23-acres
  - Road frontage of 139.75-feet and depth of the parcel 374.54-feet.
  - Latitude: 38.4032; Longitude: -122.8207
  
- 6780 Depot Street-
  - APN: 004-750-033-000
  - Partition of Birdie Miller Cnopius
  - Recorded May 7, 1918 in Book 35, Maps pages 22-00
  - Lot 34
  - 0.9-acres
  - Road frontage of 226.88-feet and irregular depth of 239.12-feet.
  - Latitude: 38.4033; Longitude: -122.8213

### Buildings

The two buildings considered in this report have several addresses; see **Figure 15** below. The building at 6782 Sebastopol Avenue also includes address numbers 6784, 6786, and 6788. The building at 6780 Depot Street also includes address numbers 6760 and 6770. The physical descriptions of the buildings are addressed separately in preparation for the separate evaluations for historical significance in the sections that follows.

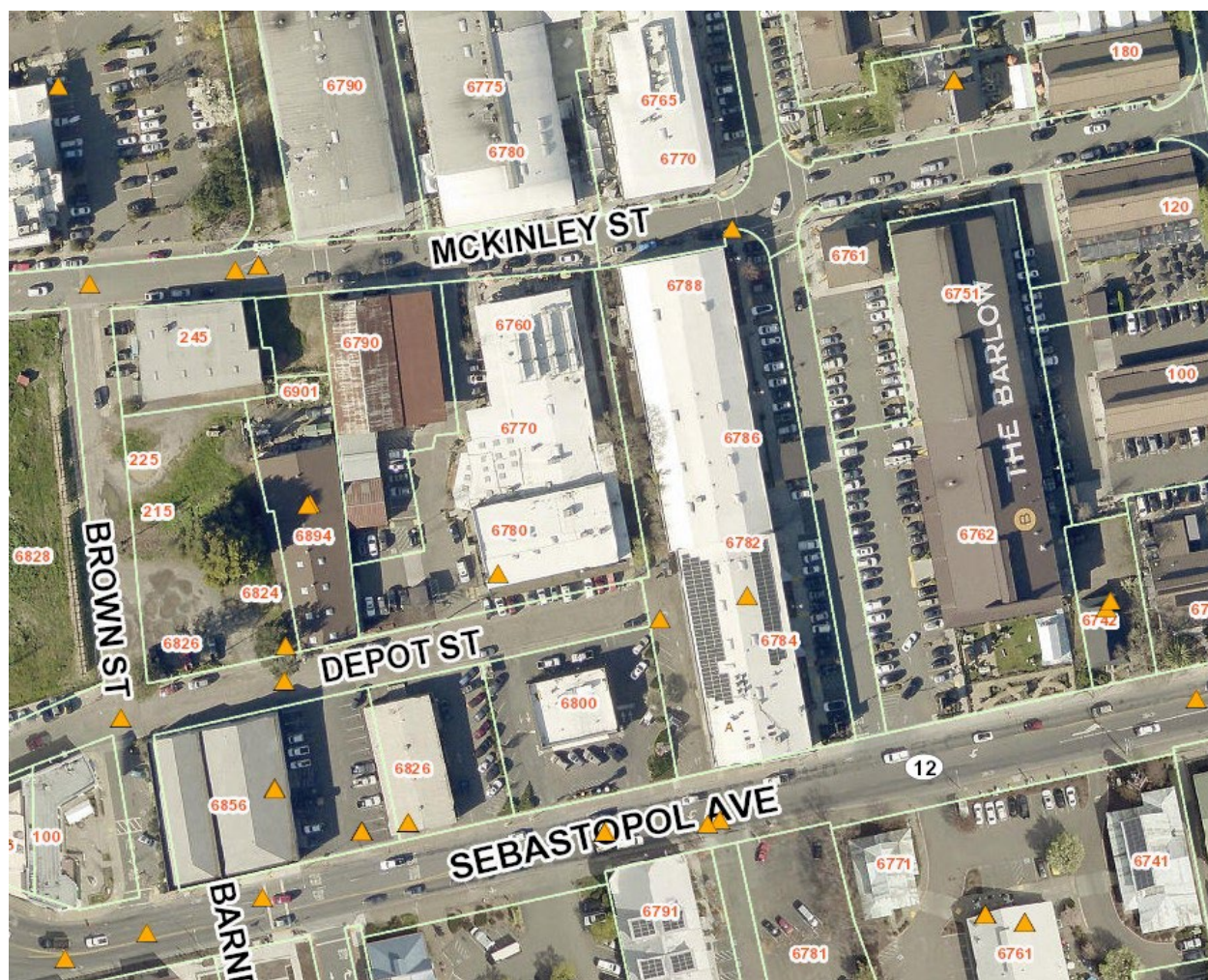
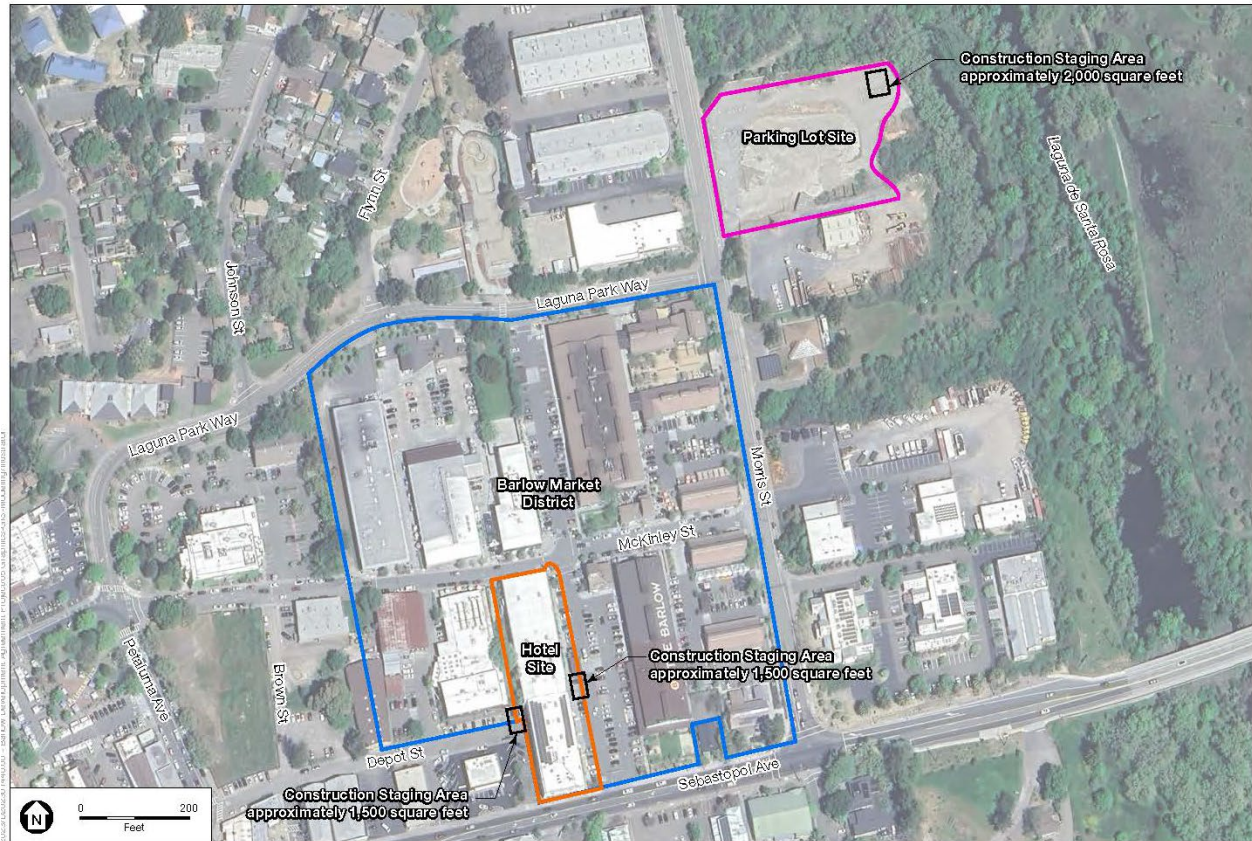


Figure 15 Parcel map of 6782 Sebastopol Avenue and 6780 Depot Street include several addresses, as seen here. Note, the orange triangles on the image indicate the location of historical photographs in the archives of the Western Sonoma County Historical Society (Courtesy of the Western Sonoma County Historical Society).



SOURCE: Aldridge Development, 2024; ESA, 2024; Google Earth, 2024

Barlow Hotel Project

Figure 166 Barlow Hotel Project property demarcated in blue boundary lines and the project APE map, as red boundary lines (Courtesy Aldridge Development and ESA from Google Earth, November 2024) Physical Description 6782 Sebastopol Avenue

6782 Sebastopol Avenue appears to date from 1924/25, as shown in the **Property History** section above. The building is a utilitarian warehouse with a raised concrete platform foundation set approximately 5-feet above grade, typical of 20<sup>th</sup>-Century railroad warehouses. The wood frame building is composed of two primary interior spaces, a 2-story office portion at the south end and a much larger handling and storage portion comprising about 80% of the 31,571-square foot interior.

The elongated rectangular plan, 390' X 81', allows for two long exterior elevations facing the railroad tracks for loading to its west and for truck loading doors to its east. The façade is fitted for pedestrian access and other fenestration for offices while the rear elevation at McKinley Street has a roll-up loading door and a second roll-up vehicular access door, leading up onto the warehouse's elevated reinforced-concrete floor.

Exterior walls feature clerestory windows at the top, which are now painted to reduce solar heat gain. The southern end of the building is clad in manufactured wood-board siding at the southern end and steel siding on the northern portion.

Standard trusses over the southern 40% and gambrel trusses over the northern 60% of the building support the roof and their span limits the building's width. The gambrel truss form is reflected in the

roof plains but terminates at the eaves with nearly flat wings that accommodate internal gutters. The roof appears to be clad in a modified bitumen roofing over sheathing.

#### *Façade – South Elevation*

The building faces south up to the sidewalk of Sebastopol Avenue. A single step parapet façade, the parapet cap is not relieved from the surface, giving an overall flat appearance. Mid- to late-20<sup>th</sup> Century cladding and flush-surface fenestration (See **Figure 16**) dates back to at least 1970, as seen in **Figure 14**. Some panels of siding are missing at the western end of the façade. Larger windows are comprised of a central fixed-window flanked by smaller sliding windows within aluminum frames. Concrete steps and steel railings lead into an inset entry with aluminum frame door. The entrance is offset to the western end of the southerly façade.



Figure 17 A view to the north-northeast (Photography by Yarbrough Architectural Resources 11/3/2023).



Figure 18 Façade with late-20th Century replacement siding, fenestration, and cornice cap (Photography by Yarbrough Architectural Resources 11/3/2023).



Figure 19 A view to the northwest (Photography by Yarbrough Architectural Resources 11/3/2023).



### East Elevation

On the east elevation, six roll-up freight doors open for truck loading but the volume of freight is mitigated by a new public access, pedestrian-friendly commercial district, The Barlow. A broad sidewalk flanks the building's east elevation and a vehicular entrance with two driveways and three rows of perpendicular parking rows now occupy the formerly industrial access area (See **Figures 18 – 21**).

Fenestration now includes both vinyl and aluminum frame windows, an exterior-mounted stairway to the second floor, and six roll-up doors, formerly devoted to truck freight access. Newer steps, railings, landscaping, and concrete hardscapes reflect the buildings and district's focus on quality and artisanal retail commodities rather than its former function for large-scale, wholesale storage and transportation.

Late-20<sup>th</sup> Century manufactured wood siding on the front, southerly portion of the east elevation retains some original clerestory windows, now painted over. The northerly portion of the east elevation is clad in corrugated steel panels.



Figure 20 (Photography by Yarbrough Architectural Resources 11/3/2023).



Figure 21 (Photography by Yarbrough Architectural Resources 11/3/2023).



Figure 22 (Photography by Yarbrough Architectural Resources 11/3/2023).

### North Elevation

The north elevation faces McKinley Street and has no parapet wall. Instead, the elevation's crest is shaped by the gambrel truss system which flatten out where the eaves to the east and west accommodate internal gutters. The elevation is clad in steel siding with two roll-up door entrances. At the eastern end, the entrance opens to a concrete ramp that rises to the reinforced concrete floor and to the right a partly glazed roll-up door provides light and freight truck access. One fixed, six-pane, wood-frame window on the elevation near the northeast corner appears to date from the building's construction in 1924/25 (See **Figures 22 – 24**).



Figure 23 (Photography by Yarbrough Architectural Resources 11/3/2023).



Figure 24 (Photography by Yarbrough Architectural Resources 11/3/2023).

#### West Elevation

The west elevation formerly faced the Northwest Pacific Railroad spur. The reinforced concrete floor platform is at the height to directly load products of the former Sebastopol Coop Cannery Company from side-doors on the west elevation onto railroad freight cars from the spur tracks that formerly paralleled the west elevation. The west elevation no longer has railroad freight access doors except for one retained where the building is accessible to truck loading access at Depot Street. By easement, a PG&E utility deck and shed addition has been added near the north end of the west elevation over an area previously occupied by railroad tracks. Like the east elevation, the west elevation includes some original clerestory windows, now painted over, and vinyl and aluminum replacement fenestration on the southern portion of the building (See **Figures 24 – 26**).

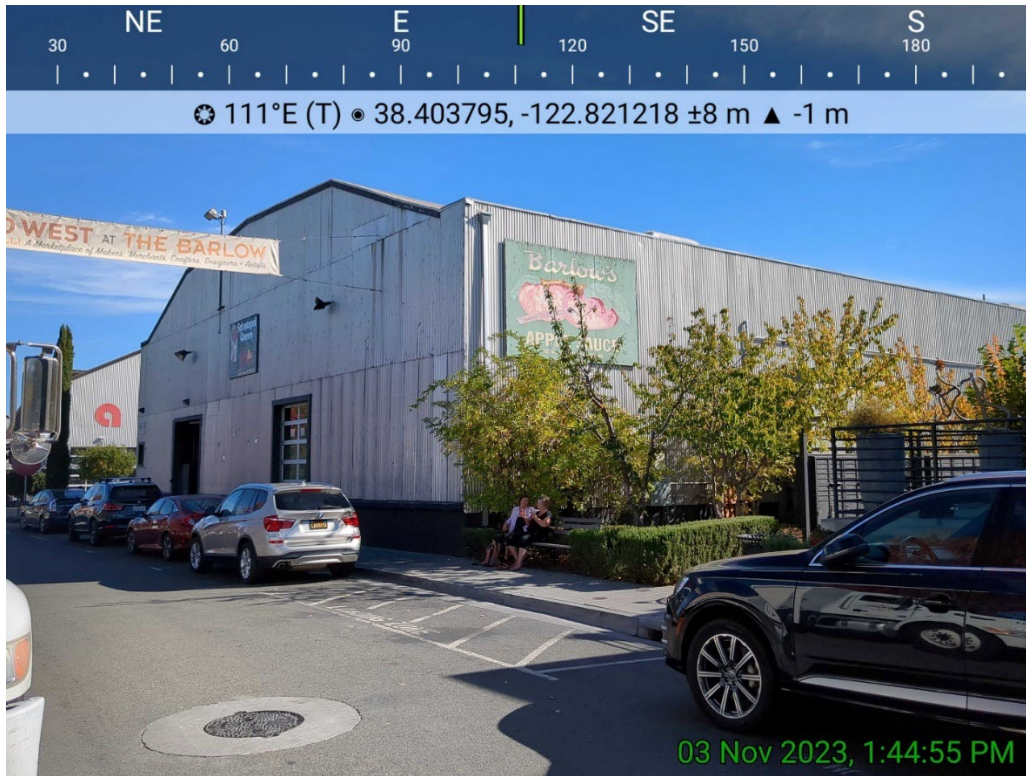


Figure 25 (Photography by Yarbrough Architectural Resources 11/3/2023).



Figure 26 (Photography by Yarbrough Architectural Resources 11/3/2023).



Figure 27 (Photography by Yarbrough Architectural Resources 11/3/2023).

### Physical Description 6780 Depot Street

6780 Depot Street is a utilitarian warehouse that appears to have been a box making and storage building for the Enterprise Bottling Works. Based on information from Sanborn Fire Insurance maps, the building was constructed between 1903 and 1911. The greatly altered building now has a newer raised concrete foundation, although the 1911 Sanborn Fire Insurance Map indicates that the building was originally on a wood foundation of some type (See **Appendix C: Sanborn Fire Insurance and Plat Maps**). The wood frame building is composed of two primary interior spaces, a 2-story office portion at the south end, that was built for box making, and a single-story storage building portion that extends back to McKinley Street. Therefore, this building is described here in two parts, the South 2-Story Unit (see **Figures 27 – 32**) and the North Unit (see **Figures 33 – 36**).

#### *South 2-Story Unit*

The south 2-story unit of 6780 Depot Street is a side-gable mass with parapet walls at each end. They feature a two-step parapet wall with a central, ridge rectangular step. The façade does not appear to retain any materials from its period of construction in the first decade of the 20<sup>th</sup>-Century. The building is clad in what appears to be a bitumen layer held in place by battens, spaced approximately 2-feet apart vertically. A concrete front porch with low-slope shed roof and concrete porch appears to date from approximately the last 40-years. The fenestration on the façade, as well as on the façade-unit of the building, are dark bronzed, aluminum-frame horizontal sliders. Of similar date, an exterior stairway with a landing and door to the second floor and contemporary office doors and windows on the first floor give the building a contemporary appearance.

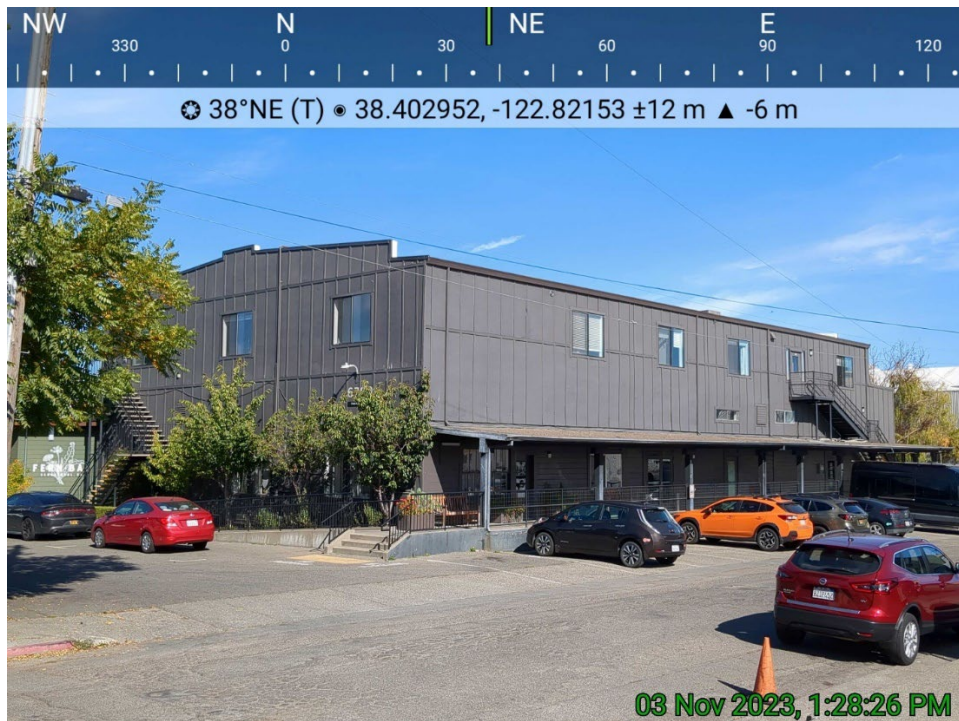


Figure 28 (Photography by Yarbrough Architectural Resources 11/3/2023).

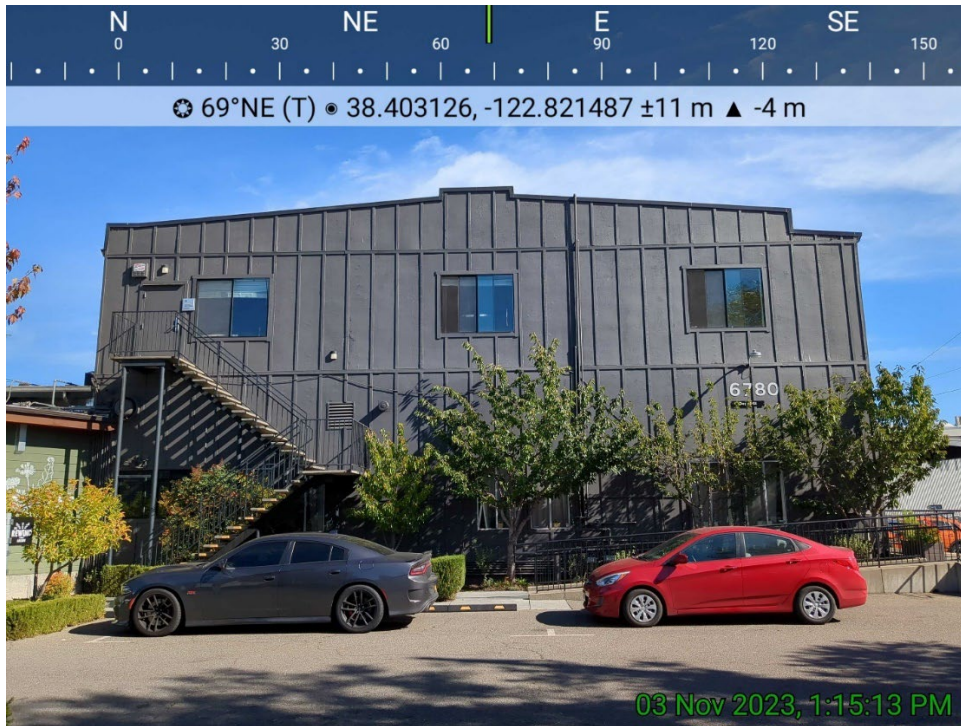


Figure 29 (Photography by Yarbrough Architectural Resources 11/3/2023).



Figure 30 (Photography by Yarbrough Architectural Resources 11/3/2023).





Figure 31 (Photography by Yarbrough Architectural Resources 11/3/2023).



Figure 32 (Photography by Yarbrough Architectural Resources 11/3/2023).



Figure 33 (Photography by Yarbrough Architectural Resources 11/3/2023).

#### North Unit

The north unit of the building is single-story parapet, gable-front building with side eaves. It is skirted with broad porches on its east, north, and west elevations. Built for storage, the roof and porch roofs are high to accommodate the movement of boxed bottles for loading on railroad freight cars on the east side of the building unit and for truck access on the west elevation. The plan is rectangular with a pushed-out addition on the east elevation at the northeast corner and another on the west elevation at the southwest corner

A secondary façade facing McKinley Street, the north unit presents a gradually sloping parapet wall with a small raised rectangular step at the ridge and flattened corner casts that appear to be a design feature remaining from its period of construction (see **Figure 33**). Between this secondary façade and McKinley Street, the building had a contemporary concrete terrace with seating and modern railing and access features.



Figure 34 View to the southeast (Photography by Yarbrough Architectural Resources 11/3/2023).



Figure 35 Note visible solar panels from McKinley Street (Photography by Yarbrough Architectural Resources 11/3/2023).



Figure 36 (Photography by Yarbrough Architectural Resources 11/3/2023)



Figure 37 (Photography by Yarbrough Architectural Resources 11/3/2023)

## Evaluation of Significance

This evaluation of 6780 Depot Street and 6782 Sebastopol Avenue as potential historical resources is conducted as an architectural or built-environment consideration of significance but does not reflect potential archaeological resources. Although this historical resource evaluation report is not developed to address a current permitting process, it is written to address the standards and guidelines of CEQA. If and when the property owner or their assigned agent may choose to pursue discretionary approval from the City of Sebastopol or other agency with regulatory authority for permitting purposes, this technical study may serve or be updated so-as to serve as a recommendation regarding the subject properties' presence or absence of sufficient historical significance. If either property is determined by the lead-agency to be a "historical resources," pursuant to CEQA or local historic preservation statutes, then limited historic preservation protections may be required by said agency. This historical resource evaluation report should not be considered as presuming to provide legal advise regarding any laws or statutes but is, rather, a technical study to inform decisionmakers.

CEQA considers properties eligible to the CRHR and to the National Register of Historic Places (NRHP) to be "historical resources." Historical resources are environmental resources and subject to certain processes and protections under the law. For a property to be an historical resource it must first qualify as significant under at least one of the criteria and retain the historical integrity to convey that significance. Therefore, this section is divided into an application of the significance criteria and followed by a consideration of the seven aspects of historical integrity.

### Study Area

The Study Area is comprised of the current boundaries of the properties known as 6780 Depot Street, Assessor's Parcel Number (APN) 004-750-033-000 and 6782 Sebastopol Road (APN) 004-750-030-000. The study area does not address a potential project in this technical study, such as a CEQA Study Area, but defines the identification and evaluation limits of this study, the sole purpose of which is to recommend whether the property is a "historical resource" pursuant to CEQA guidelines.

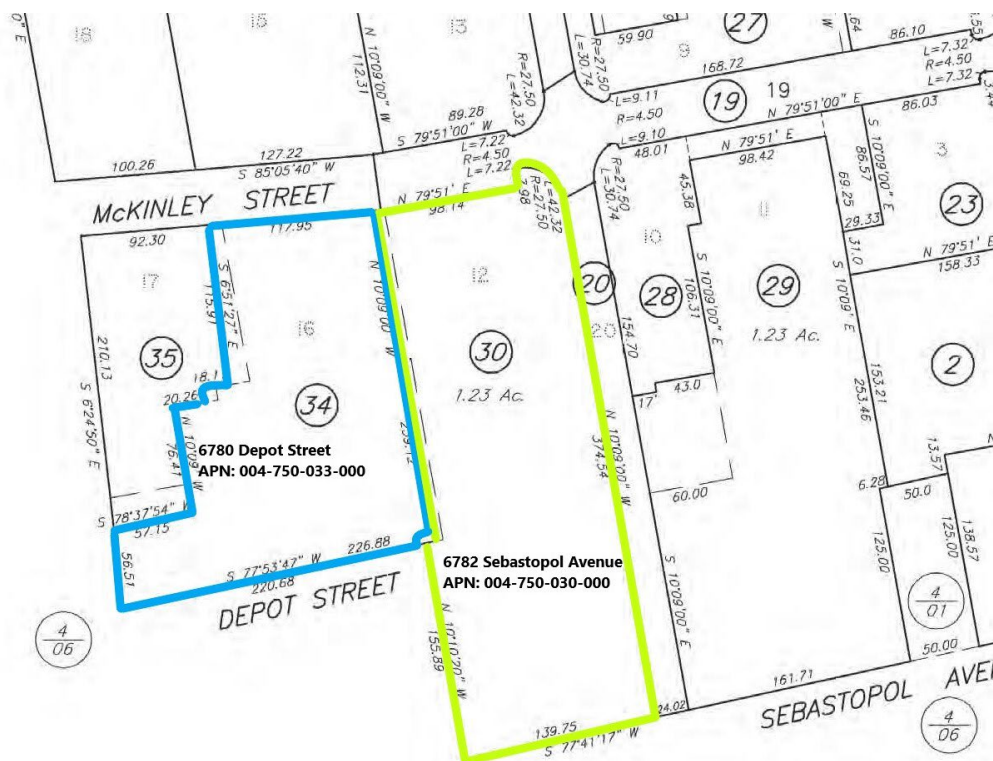


Figure 38 Study Area of two adjacent parcels, outlined and labelled, from a Sonoma County Assessor's Map.

### Historic District Consideration

California Public Resources Code Section 5020.1(h) defines an "historic district" as:

"Historic district" means a definable unified geographic entity that possesses a significant concentration, linkage, or continuity of sites, buildings, structures, or objects united historically or aesthetically by plan or physical development." (California Public Law 2023)

The following inquiry was communicated by Edward Yarbrough, Architectural Historian and Principal of Yarbrough Architectural Resources to Mr. J. Jay, Planner, Planning & Community Development Department, City of Sebastopol and copying Planning Director, Kari Svanstrom. Edward Yarbrough addressed the following by email to Planner J. Jay:

"I have been retained to write a Historical Resources Evaluation Report (HRER) for two buildings in the Barlow and am writing to request guidance and perhaps examples of HRERs that have met the standards of the City's Planning Department. Anything that you can share or guidance that you can provide would be greatly appreciated." [Email from Yarbrough to Jay, Nov. 3, 2023].

No response was received as of the completion of this technical study. Although Yarbrough Architectural Resources recommends that no eligible historic district is present. Only the City of Sebastopol, in any future role as lead-agency for both CEQA and municipal compliance, can determine whether or not an eligible historic district is present and, if so, whether or not the subject property would be considered a contributing resource to a historic district. Lead agencies may find eligible districts during the historical review process for individual properties. However, a technical analysis regarding a potential historic

district is beyond the scope of this study and can only be determined by the City of Sebastopol or a state or federal agency. Determining the presence of an eligible but unlisted historic district is based on previous historical resource evaluations, historical surveys, or by consideration of an eligible historic district during a formal review process.

## Application of the Significance Criteria to 6782 Sebastopol Avenue

The application of criteria to determine the eligibility of 6782 Sebastopol Avenue is conducted for the purpose of recommending whether the property is individually significant, as defined by CEQA guidelines. The four CRHR significance criteria closely mirror those of the NRHP and are routinely applied in tandem to one another to evaluate resources subject to CEQA and municipal compliance. The NRHP and CRHR recommendation in this study follow that tandem evaluation approach. NRHP criteria A, B, C & D and CRHR criteria 1, 2, 3 & 4 are jointly considered as Criterion A/1, referring to NRHP Criterion A and CRHR Criterion 1, as follows:

### NRHP Criterion A/CRHR Criterion 1 – *Not Recommend as Eligible*

To qualify for listing under Criterion A/1 of the NRHP/CRHR, a resource must be identified with an important event in history, an important pattern of events, or be significantly representative of a period of development. In review of historical documentation of 6782 Sebastopol, the former cannery railroad warehouse was not found to be mentioned in connection with a significant historical event. The warehouse was owned and operated by the Sebastopol Coop Cannery, earlier referred to as the Sebastopol Cooperative Canning Company, from which apple and other fruits were shipped as final products, the building is one of several apple, particularly Gravenstein apple, processing and product storage and shipping related structures at the eastern end of the City. No documentation of a known-eligible historic district is recorded here by the City of Sebastopol and the West Sonoma County Historic Resource Survey of 1981, revised in 2023, did not include the warehouse or neighboring warehouses worthy of inclusion or consideration. The aforementioned survey assessed and found historical resources within the blocks of the warehouse but did not include 6782 Sebastopol Avenue. The warehouse, built circa 1924/25, reflects Sebastopol's history of fruit packing and shipment by railroad but does not appear to be a significant representation of that period in the community's development. What associations the railroad warehouse building had with Sebastopol's renowned apple industry are overshadowed by the extant Gravenstein orchards, Luther Burbank experimental farm, and other character-defining structures and landscapes related to the development of the Gravenstein apple industry. Therefore, this resource is not recommended as eligible to the NRHP under Criterion A nor to the CRHR under Criterion 1.

### NRHP Criterion B/CRHR Criterion 2 – *Not Recommend as Eligible*

To qualify for listing under Criterion B/2 of the NRHP/CRHR, a resource must be identified with a person important in history. The founders of the Sebastopol Coop Cannery were Newton B. Kinley of Santa Rosa, Fred K. Hammell of Petaluma, Wendell Henderson of Kelseyville, and I. N. Cable, W. O. Barnes, James Blundin, and Henry Elphick of Sebastopol. These organizers and shareholders of the cannery do not appear to be persons of particular historical significance through their ownership relationship with the warehouse building. With particular attention to the four investors hailing from Sebastopol, namely Cable, Barnes, Blundin, and Elphick, the names did not appear in newspaper nor Western Sonoma County Historical Society articles or other online searches. Therefore, this resource is not recommended as eligible to the NRHP under Criterion B nor to the CRHR under Criterion 2.

### NRHP Criterion C/CRHR Criterion 3 – *Not Recommend as Eligible*

To qualify for listing under Criterion C/3 of the NRHP/CRHR, a resource must be identified with important movements in, or masters of, design and construction or as representative of an historically significant architectural type. This resource, built circa 1924/25, is illustrative of a railroad warehouse type that was



well established in the California by the 1920s and does not represent innovation or mastery in design or construction. Therefore, this resource is not recommended as eligible to the NRHP under Criterion C nor to the CRHR under Criterion 3.

#### NRHP Criterion D/CRHR Criterion 4– *Not Recommend as Eligible* (Archaeological Resources not considered herein)

To qualify for listing under Criterion D/4 of the NRHP/CRHR, a resource must have yielded or be likely to yield information important to prehistory or history. This study does not include the consideration of archaeological resources or of prehistory. However, as a built-environment or above-ground resource, the subject warehouse is a common structural form and does not embody information that may answer an unresolved historical question regarding design, construction, or history. Built forms of this type are well documented in current scholarship of building engineering and technology. Therefore, this resource is not recommended as eligible to the NRHP under Criterion D nor to the CRHR under Criterion 4.

#### Period of Significance

Only properties that are recommended as individually eligible under at least one of the above criteria are historically significant and, therefore, capable of having a period of significance. Individually, this property, consisting of a parcel with a railroad warehouse, is not recommended as eligible to the NRHP or CRHR. Although the warehouse was built after the Sebastopol Coop Cannery Company's organization in 1924 and constructed soon thereafter, the property is recommended as ineligible under any NRHP and CRHR criteria in the section above and, therefore, has no historical significance that can be associated with a certain historical period.

#### Historical Integrity Assessment

The Department of Interior, National Park Service recognizes seven aspects of historical integrity, that of location, setting, design, workmanship, materials, feeling, and association. Only NRHP- or CRHR-eligible properties have features that require retention to convey that significance. 6782 Sebastopol Avenue is not recommended as significant against NRHP and CRHR criteria A/1, B/2, C/3, or, excluding archaeological evaluation, D/4. Therefore, the property does not possess character-defining features which would otherwise be associated with its significance.

### Application of the Significance Criteria to 6780 Depot Street

The application of criteria to determine the eligibility of 6780 Depot Street is conducted for the purpose of recommending whether the property is individually significant, as defined by CEQA guidelines. The four CRHR significance criteria closely mirror those of the NRHP and are routinely applied in tandem to one another to evaluate resources subject to CEQA and municipal compliance. The NRHP and CRHR recommendation in this study follow that tandem evaluation approach. NRHP criteria A, B, C & D and CRHR criteria 1, 2, 3 & 4 are jointly considered as Criterion A/1, referring to NRHP Criterion A and CRHR Criterion 1, as follows:

#### NRHP Criterion A/CRHR Criterion 1 – *Not Recommend as Eligible*

To qualify for listing under Criterion A/1 of the NRHP/CRHR, a resource must be identified with an important event in history. In review of historical documentation of Sebastopol's development and the area where cannery warehouses are present adjacent to rail and road networks, 6780 Depot Street was not found to be mentioned in connection with a significant historical event. Although the extant warehouse was constructed between 1903 and 1911, as shown on Sanborn Fire Insurance maps of those dates, the building is part of a pattern of industrial development in the area that began at the turn of the 20<sup>th</sup> Century and continued into the 1970s. The property was subject to historical inventory and review, as recorded in the *Western Sonoma County Historic Resources Survey, Volume 2. The City of Sebastopol* (1981; reprinted and revised 2023) and specifically excluded from the list of historical resources listed therein. This Historic Resource Survey included properties with significance due to association with important historical events and associations. The warehouse building and its property are not associated with an important historical event or exceptionally representative of the City's industrial or transportation development. Therefore, this resource is not recommended as eligible to the NRHP under Criterion A nor to the CRHR under Criterion 1.

#### NRHP Criterion B/CRHR Criterion 2 – *Not Recommend as Eligible*

To qualify for listing under Criterion B/2 of the NRHP/CRHR, a resource must be identified with a person important in history. No individual of historical significance in the history of Sebastopol could be identified with the subject property. The property was considered in the historical inventory and review, as recorded in the *Western Sonoma County Historic Resources Survey, Volume 2. The City of Sebastopol* (1981; reprinted and revised 2023) and specifically excluded from the list of historical resources listed therein. This Historic Resource Survey included properties with significance due to association with an important individual in history. The warehouse building and its property are not associated with an important individual associated with the City's development. Therefore, this resource is not recommended as eligible to the NRHP under Criterion B nor to the CRHR under Criterion 2.

#### NRHP Criterion C/CRHR Criterion 3 – *Not Recommend as Eligible*

To qualify for listing under Criterion C/3 of the NRHP/CRHR, a resource must be identified with important movements in, or masters of, design and construction or as representative of an historically significant architectural type. This resource is illustrative of a railroad warehouse type that was well established in the California by the 1920s and does not represent innovation or mastery in design or construction. Therefore, this resource is not recommended as eligible to the NRHP under Criterion C nor to the CRHR under Criterion 3.

#### NRHP Criterion D/CRHR Criterion 4– *Not Recommend as Eligible* (Archaeological Resources not considered herein)

To qualify for listing under Criterion D/4 of the NRHP/CRHR, a resource must have yielded or be likely to yield information important to prehistory or history. This study does not include the consideration of archaeological resources or of prehistory. However, as a built-environment or above-ground resource, the subject warehouse is a common structural form and does not embody information that may answer an unresolved historical question regarding design, construction, or history. Built forms of this type are well documented in current scholarship of building engineering and technology. Therefore, this resource is not recommended as eligible to the NRHP under Criterion D nor to the CRHR under Criterion 4.

#### Period of Significance

Only properties that are recommended as individually eligible under at least one of the above criteria are historically significant and, therefore, capable of having a period of significance. Individually, this property, consisting of a parcel with a railroad warehouse, is not recommended as eligible to the NRHP or CRHR. Although the warehouse was built sometime in the first decade of the 20<sup>th</sup>-Century, the property is recommended as ineligible under any NRHP and CRHR criteria in the section above and, therefore, has no historical significance that can be associated with a certain historical period.

#### Historical Integrity Assessment

The Department of Interior, National Park Service recognizes seven aspects of historical integrity, that of location, setting, design, workmanship, materials, feeling, and association. Only NRHP- or CRHR-eligible properties have features that require retention to convey that significance. 6780 Depot Street is not recommended as significant against NRHP and CRHR criteria A/1, B/2, C/3, or, excluding archaeological evaluation, D/4. Therefore, the property does not possess character-defining features which would otherwise be associated with its significance.

## Conclusions and CEQA Impacts Analysis

The warehouses at 6782 Sebastopol Avenue and 6780 Depot Street are not recommended as individually eligible to the NRHP under criteria A, B, C, or D nor to the CRHR under criteria 1, 2, 3, or 4. This historic resource evaluation report does not consider potential eligibility under NRHP criterion D nor CRHR criterion 4 for purposes of archaeological resources. Therefore, this technical study recommends that the property is not a historical resource, as defined under CEQA.

The property is not within a listed historic district. This technical study does not recommend whether the property is within an eligible historic district, which is beyond the scope of this report.

In this December 2024 HRER revision, the Barlow Hotel Project encompasses the property at 6782 Sebastopol Avenue but does not include 6780 Depot Street in the APE. Although this technical study continues to provide a recommendation of ineligibility and lack of historical significance for 6780 Depot Street, the HRER now identifies, recommends ineligibility, and therefore, a CEQA finding of *no historical resources impacted* for the property within the APE, namely 6782 Sebastopol Avenue. The APE does not include areas of indirect impact or temporary construction staging areas.

### CEQA Finding Recommendation

Regarding the property located within the CEQA Study Area or APE, namely 6782 Sebastopol Avenue, this technical study recommends a finding of *No historical resources impacted*.

## References Cited

California Public Law (website)

2023 CA Pub Res Code Section 5020.1 website  
[https://california.public.law/codes/ca\\_pub\\_res\\_code\\_section\\_5020.1](https://california.public.law/codes/ca_pub_res_code_section_5020.1), accessed 10/30/2023.

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Hansen, Harvey J., Jeanne Thurlow Miller, and David Wayne Peri

1962 *Wild Oats in Eden: Sonoma County in the 19<sup>th</sup> Century*. Self-published, Kenwood, California.

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2023 Yarbrough Architectural Resources to , Senior Planner, Planning and Community Development Department. November 3, 2023.

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2023 Laguna de Santa Rosa page at [sonomacounty.com](https://sonomacounty.com). Accessed November 27, 2023.

Western Sonoma County Historical Society

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Yarbrough Architectural Resources

2023 *Cultural Landscape Report: At Crossover Road, Olema Bolinas Road & California State Route 1*. Bolinas Lagoon Wye Wetlands Resiliency Project, Marin County Parks, Marin County, California. November 2023.

## Appendix A: DPR Form Sets

State of California  The Resources Agency  
 DEPARTMENT OF PARKS AND RECREATION  
**PRIMARY RECORD**

Primary #  
 HRI #  
 Trinomial  
**NRHP Status Code**

Other  
 Review Code

Reviewer

Date

Listings

Page 1 of 22 \*Resource Name or #: 6782 Sebastopol Avenue, Sebastopol, CA

P1. Other Identifier: Former Sebastopol Coop Cannery Company Warehouse

\*P2. Location:  Not for Publication  Unrestricted

\*a. County Sonoma

\*b. USGS 7.5' Quad Sebastopol Date 2018-09-05 T 11S; R 1W;  of  of Sec 18;      B.M.

c. Address 6782 Sebastopol Ave City Sebastopol Zip 95472

d. UTM: Zone 38.402833070744755, -122.82064201238359 e. Other Locational Data: Assessor's Parcel Number (APN): 004-750-030-000

\*P3a. Description: The building is a utilitarian warehouse with a raised concrete platform foundation set approximately 5-feet above grade, typical of 20<sup>th</sup>-Century railroad warehouses. (See DPR 523I continuation sheet page 3)

\*P3b. Resource Attributes: Industrial Building HP31.

\*P4. Resources Present:  Building  
 Structure  Object  Site  District   
 Element of District  Other

P5b. Description of Photo: View to NNE, 3 Nov 2023, photo by Yarbrough Architectural Resources

\*P6. Date Constructed/Age and Source:  Historic  
Ca. 1925, newspaper article Santa Rosa Press Democrat

\*P7. Owner and Address:

Barlow Star LLC  
6780 Depot Street, Suite 7  
Sebastopol, CA 95472

\*P8. Recorded by: Edward Yarbrough, Yarbrough Architectural Resources, 2150 Silverado Trl N, Saint Helena, CA 94574

\*P9. Date Recorded: Jan. 14, 2024

\*P10. Survey Type: (Describe)  
 Individual property  
 Historical Resource  
Evaluation Report for Constraints Analysis

P5a. Photograph or Drawing



\*P11. Report Citation: (Cite survey report and other sources, or enter "none.")

HISTORICAL RESOURCE EVALUATION REPORT: Constraints Analysis for 6780 Depot St. & 6782 Sebastopol Ave., Sebastopol, Sonoma County, California Yarbrough Architectural Resources, January 2024

\*Attachments:  NONE  Location Map  Continuation Sheet  Building, Structure, and Object Record

Archaeological Record  District Record  Linear Feature Record  Milling Station Record  Rock Art Record

Artifact Record  Photograph Record  Other (List): \_\_\_\_\_



\*Resource Name or # (Assigned by recorder) \_\_\_\_\_ \*NRHP Status Code 6Z  
 Page 2 of 20

B1. Historic Name: Enterprise Bottling Works Warehouse Building  
 B2. Common Name: 6780 Depot Street  
 B3. Original Use: Railroad Warehouse B4. Present Use: Commercial Rental Property \*B5.  
 Architectural Style: Industrial Railroad Warehouse circa 1903 - 1911  
 \*B6. Construction History: (Construction date, alterations, and date of alterations)

(See DPR 5231 Continuation Sheet beginning at page 14 of this form set)

\*B7. Moved?  No  Yes  Unknown

\*B8. Related Features: The building is part of a 12-acre district of adaptively reused industrial and railroad buildings called The Barlow.

B9a. Architect: unknown b. Builder: unknown

\*B10. Significance: Theme None Area The Barlow retail and commercial district  
 Period of Significance None Property Type Industrial Building HP31 Applicable Criteria None

(See DPR 5231 Continuation Sheet beginning at page 18 of this form set)

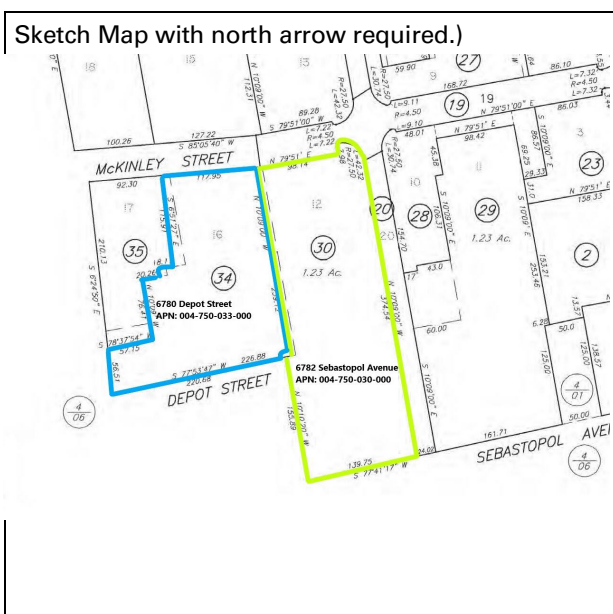
B11. Additional Resource Attributes: (List attributes and codes) None

\*B12. References:  
 (See DPR 5231 Continuation Sheet beginning at page 20 of this form set)

B13. Remarks:  
 Edward Yarbrough, a qualified architectural historian, conducted a site visit with photographs and notes on November 3, 2023 and research at the Sonoma County Recorder's Office, Sebastopol Public Library, Western Sonoma County Historical Society, as well as numerous other online sources of information.

\*B14. Evaluator: Edward Yarbrough, Architectural Historian & Principal, Yarbrough Architectural Resources  
 \*Date of Evaluation: January 2024

(This space reserved for official comments.)



## CONTINUATION SHEET

Property Name: \_\_\_\_\_  
Page \_\_\_\_ of \_\_\_\_

Page 3 of 22 \*Resource Name or # (Assigned by recorder)

\*Recorded by: Edward Yarbrough \*Date Jan. 14, 2024  Continuation

### P3a. Description

#### Parcel

- 6782 Sebastopol Avenue-
  - APN: 004-750-030-000
  - Partition of Birdie Miller Cnopius
  - Recorded May 7, 1918 in Book 35, Maps pages 22-00
  - Lot 30
  - 1.23-acres
  - Road frontage of 139.75-feet and depth of the parcel 374.54-feet.
  - Latitude: 38.4032; Longitude: -122.8207

#### Building

6782 Sebastopol Avenue appears to date from 1924/25, as shown in the **Property History** section above. The building is a utilitarian warehouse with a raised concrete platform foundation set approximately 5-feet above grade, typical of 20<sup>th</sup>-Century railroad warehouses. The wood frame building is composed of two primary interior spaces, a 2-story office portion at the south end and a much larger handling and storage portion comprising about 80% of the 31,571-square foot interior.

The elongated rectangular plan, 390' X 81', allows for two long exterior elevations facing the railroad tracks for loading to its west and for truck loading doors to its east. The façade is fitted for pedestrian access and other fenestration for offices while the rear elevation at McKinley Street has a roll-up loading door and a second roll-up vehicular access door, leading up onto the warehouse's elevated reinforced-concrete floor.

Exterior walls feature clerestory windows at the top, which are now painted to reduce solar heat gain. The southern end of the building is clad in manufactured wood-board siding at the southern end and steel siding on the northern portion.

Standard trusses over the southern 40% and gambrel trusses over the northern 60% of the building support the roof and their span limits the building's width. The gambrel truss form is reflected in the roof plains but terminates at the eaves with nearly flat wings that accommodate internal gutters. The roof appears to be clad in a modified bitumen roofing over sheathing.

## CONTINUATION SHEET

Property Name: \_\_\_\_\_  
Page \_\_\_\_ of \_\_\_\_

Page 4 of 22 \*Resource Name or # (Assigned by recorder)  
\*Recorded by: Edward Yarbrough \*Date Jan. 14, 2024  Continuation

### *Façade – South Elevation*

The building faces south up to the sidewalk of Sebastopol Avenue. A single step parapet façade, the parapet cap is not relieved from the surface, giving an overall flat appearance. Mid- to late-20<sup>th</sup> Century cladding and flush-surface fenestration (See **Figure 1**) dates back to at least 1970, as seen in the **Construction History** section. Some panels of siding are missing at the western end of the façade. Larger windows are comprised of a central fixed-window flanked by smaller sliding windows within aluminum frames. Concrete steps and steel railings lead into an inset entry with aluminum frame door. The entrance is offset to the western end of the southerly façade.



Figure 1 A view to the north-northeast (Photography by Yarbrough Architectural Resources 11/3/2023).

# CONTINUATION SHEET

Property Name: \_\_\_\_\_  
Page \_\_\_\_ of \_\_\_\_

Page 5 of 22 \*Resource Name or # (Assigned by recorder)  
\*Recorded by: Edward Yarbrough \*Date Jan. 14, 2024  Continuation



Figure 2 Façade with late-20th Century replacement siding, fenestration, and cornice cap (Photography by Yarbrough Architectural Resources 11/3/2023).

## CONTINUATION SHEET

Property Name: \_\_\_\_\_  
Page \_\_\_\_ of \_\_\_\_

Page 6 of 22 \*Resource Name or # (Assigned by recorder)  
\*Recorded by: Edward Yarbrough \*Date Jan. 14, 2024  Continuation



Figure 3 A view to the northwest (Photography by Yarbrough Architectural Resources 11/3/2023).

### East Elevation

On the east elevation, six roll-up freight doors open for truck loading but the volume of freight is mitigated by a new public access, pedestrian-friendly commercial district, The Barlow. A broad sidewalk flanks the building's east elevation and a vehicular entrance with two driveways and three rows of perpendicular parking rows now occupy the formerly industrial access area (See **Figures 4 – 6**).

Fenestration now includes both vinyl and aluminum frame windows, an exterior-mounted stairway to the second floor, and six roll-up doors, formerly devoted to truck freight access. Newer steps, railings, landscaping, and concrete hardscapes reflect the buildings and district's focus on quality and artisanal retail commodities rather than its former function for large-scale, wholesale storage and transportation.

Late-20<sup>th</sup> Century manufactured wood siding on the front, southerly portion of the east elevation retains some original clerestory windows, now painted over. The northerly portion of the east elevation is clad in corrugated steel panels.

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Figure 4 (Photography by Yarbrough Architectural Resources 11/3/2023).

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Figure 5 (Photography by Yarbrough Architectural Resources 11/3/2023).

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Figure 6 (Photography by Yarbrough Architectural Resources 11/3/2023).

### North Elevation

The north elevation faces McKinley Street and has no parapet wall. Instead, the elevation's crest is shaped by the gambrel truss system which flattens out where the eaves to the east and west accommodate internal gutters. The elevation is clad in steel siding with two roll-up door entrances. At the eastern end, the entrance opens to a concrete ramp that rises to the reinforced concrete floor and to the right a partly glazed roll-up door provides light and freight truck access. One fixed, six-pane, wood-frame window on the elevation near the northeast corner appears to date from the building's construction in 1924/25 (See **Figures 7 - 9**).



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Figure 7 (Photography by Yarbrough Architectural Resources 11/3/2023).

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Figure 8 (Photography by Yarbrough Architectural Resources 11/3/2023).

### *West Elevation*

The west elevation formerly faced the Northwest Pacific Railroad spur. The reinforced concrete floor platform is at the height to directly load products of the former Sebastopol Coop Cannery Company from side-doors on the west elevation onto railroad freight cars from the spur tracks that formerly paralleled the west elevation. The west elevation no longer has railroad freight access doors except for one retained where the building is accessible to truck loading access at Depot Street. By easement, a PG&E utility deck and shed addition has been added near the north end of the west elevation over an area previously occupied by railroad tracks. Like the east elevation, the west elevation includes some original clerestory windows, now painted over, and vinyl and aluminum replacement fenestration on the southern portion of the building (See **Figures 9 - 11**).

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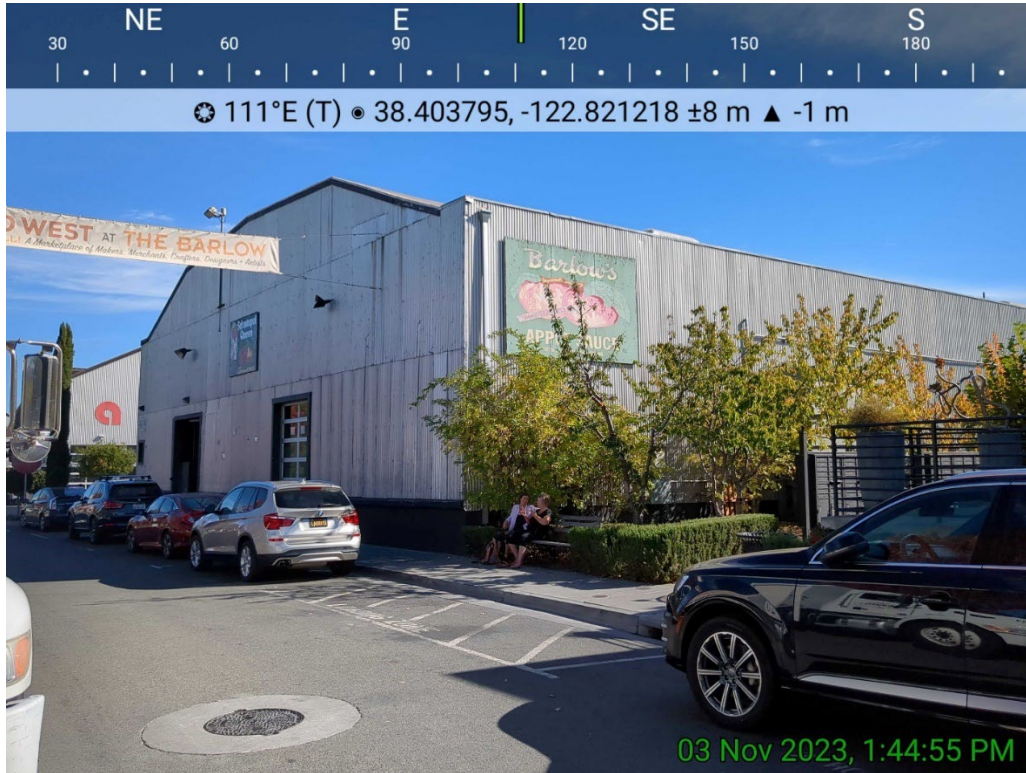


Figure 9 (Photography by Yarbrough Architectural Resources 11/3/2023).

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Figure 10 (Photography by Yarbrough Architectural Resources 11/3/2023).

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Figure 11 (Photography by Yarbrough Architectural Resources 11/3/2023).

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### B6. Construction History:

#### Property History

##### *Summary of Property Identification*

The warehouse at 6782 Sebastopol Avenue was built circa 1924/1925 by the Sebastopol Cooperative Canning Company. No records of the architect or builder were identified.

##### *Evidence of Property History*

The land possessed agricultural advantages and, later, suitability for light industrial uses: rich silt and loam soils to support the mineral and organic needs of orchards and other crops, such as at John and Barbara Brown's farm that preceded construction of the railroad warehouses (see **figures 7 & 8**); a level surface for agricultural access roads and later for railroad tracks; and, access at the intersection of downtown, once vast orchards, and proximity to larger markets and transportation networks to the east of Sebastopol (see **figures 9 & 10**).

At the time of the 1911 Sanborn Insurance Map both sides of the block where 6780 Depot Street sits included 5-residences, a Roman Catholic Church, and the H. H. Laton finished lumber warehouse and lumber yard.

The fruit processing and canning buildings and railroad spurs that connected them to distant markets were built west of the Laguna de Santa Rosa wetlands between 1903 and 1911, according to Sanborn Fire Insurance maps from those dates; see **Appendix C: Sanborn Fire Insurance and Plat Maps and Aerial Photographs**. The first railroad warehouses recorded in the area by 1911 were the Italian-Swiss Colony Winery & Distillery, the Central California Canneries Company, and a box-making and warehouse that may have been the Enterprise Bottling Works at 6780 Depot Street. These structures were connected to railroad track spurs from the Western Pacific Railroad line, a company formed in 1903 to compete with the Southern Pacific Railroad monopoly (Western Sonoma County Historical Society 2003).

6780 Depot Street primary unit, now the 2-story façade with front porch, is labeled as a space for "box making" and had a partial deck along the eastern portion of the façade. The front portion is labeled as having wood or post foundations while the rear extension that runs back to McKinley Street had a concrete perimeter wall.

In 1911, the property that would become 6782 Sebastopol Avenue (the avenue also previously referred to as both Santa Rosa Avenue and Santa Rosa Road) was bisected by Depot Street. No building appears at 6782 Sebastopol Avenue on that date.

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On page A-1 of the 1918 Grantee/Grantor Book, Sonoma County Assessor's Official Records, Nulaid Farmers Association sold the parcel at what would become 6782 Sebastopol Avenue to the Sebastopol Cooperative Cannery on August 28, 1918. The extant building on the parcel had not yet been constructed. On September 18, 1924, a short article in the Santa Rosa Press Democrat (see **Figure 12**) announced that the Sebastopol Coop Cannery Company was organized with 4,000-shares by "Newton B. Kinley of Santa Rosa, I. N. Cable, W. O. Barnes, James Blundin, Henry Elphick, Sebastopol; Fred K. Hammell, Petaluma, and Wendell Henderson, Kelseyville." Further, the article announces, "the company plans to erect a modern cannery for the preserving of fruits, vegetable and berries grown in Sonoma County." With this evidence, the date of construction for the Sebastopol Coop Cannery building at 6782 Sebastopol Avenue was likely begun in 1924 and completed in 1925.

The initial investors listed in the newspaper article (see **Figure 11**) did not appear in further research to be historically significant individuals. The four investors hailing from Sebastopol, namely Cable, Barnes, Blundin, and Elphick, did not appear in other newspaper searches nor in Western Sonoma County Historical Society resources online.

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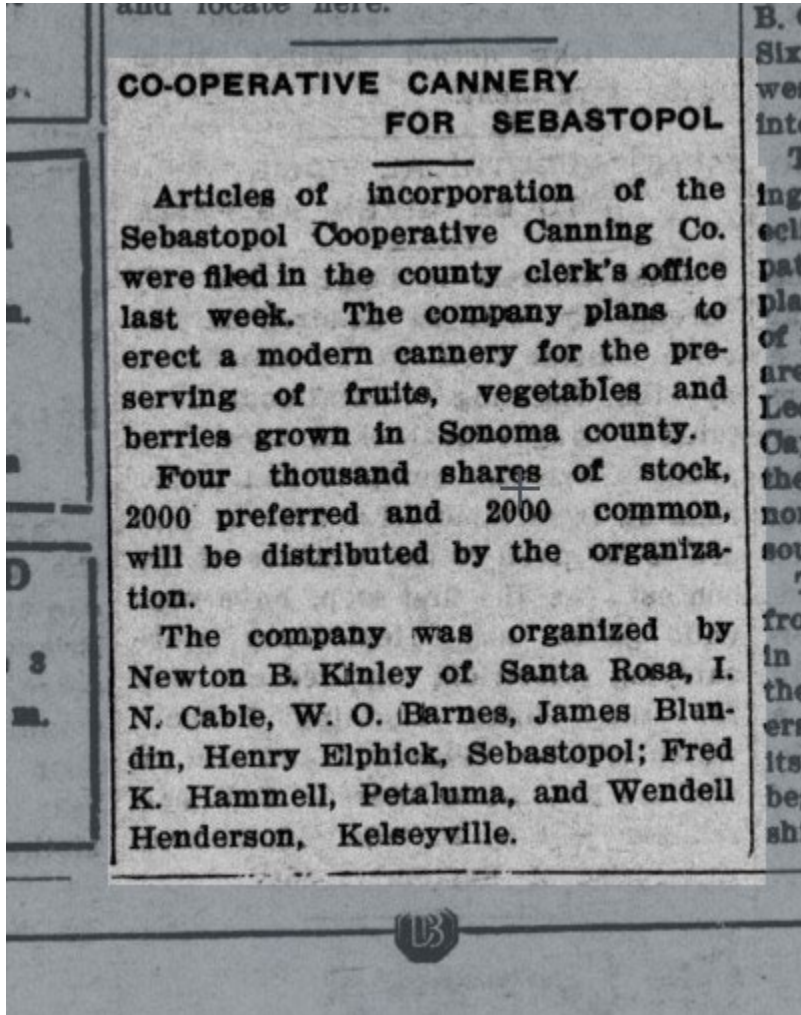


Figure 12 "Plans to erect a modern cannery for the preserving of fruits, vegetables and berries grown in Sonoma County" by the newly organized Sebastopol Cooperative Cannery Company were announced on September 18, 1924 in the Santa Rosa Press Democrat newspaper.

In **Figure 13**, in the center of the 1965 aerial photograph, 6780 Depot Street is the building directly above the elongated form of 6782 Sebastopol Avenue.

Railroad spurs and access to main lines were increasingly abandoned in the County as products were shipped by truck, a trend that accelerated following World War II. However, railroad transport continued to be a primary method of shipping product from the Sebastopol Coop Cannery's warehouse. For example, the Sebastopol Coop Cannery shipped four entire railroad freight cars of Apple Time juices and sauces to market weekly, according to the *Santa Rosa Press Democrat* newspaper in 1968 (See **Figure 13**)



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Figure 13 Aerial photo taken from the east of the subject properties in 1965 (Courtesy of the online photographic archives collections of the Western Sonoma County Historical Society, accessed via the Sonoma County Library).

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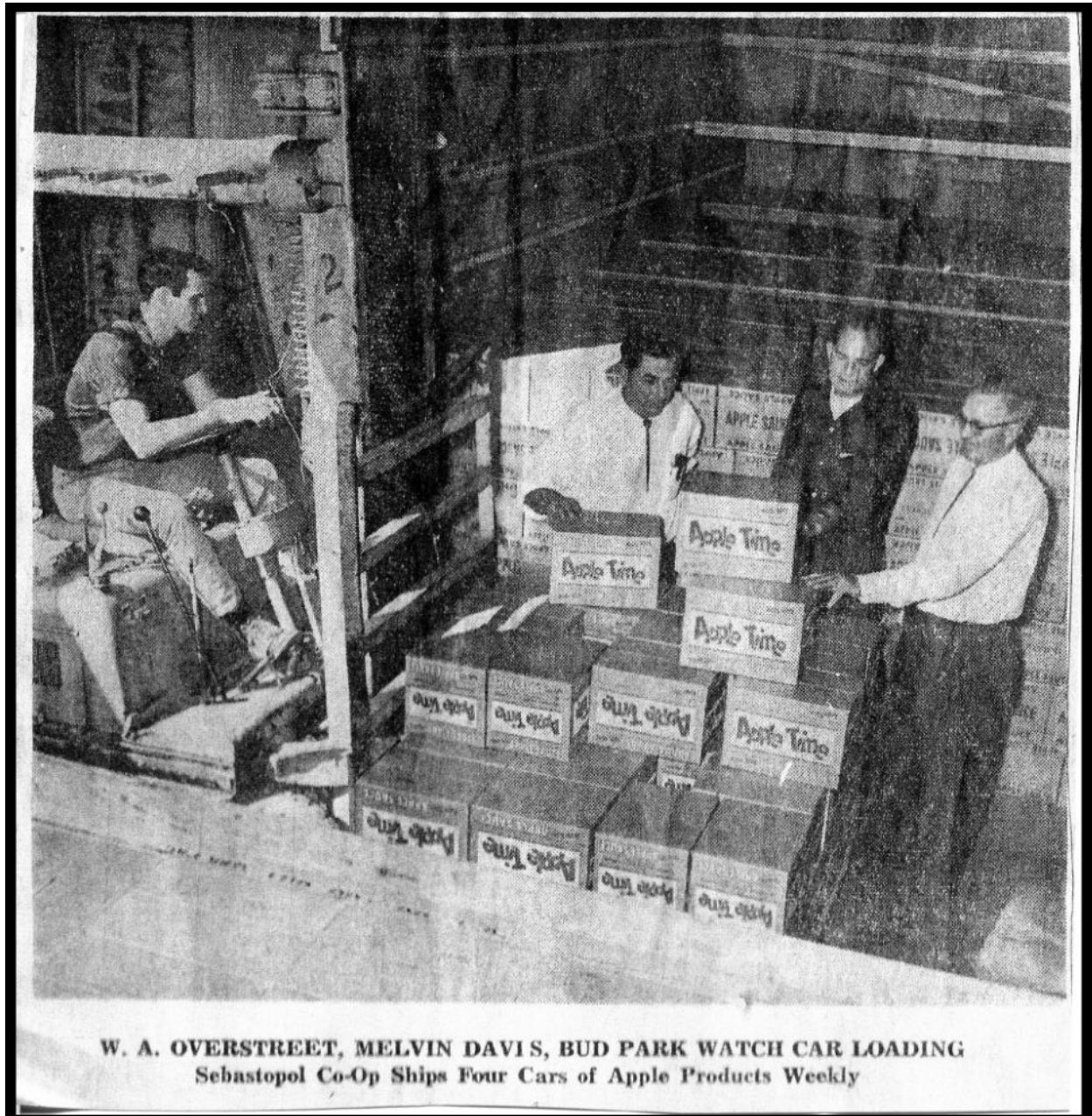


Figure 14 From the Santa Rosa Press Democrat, a forklift driver and three managers of the Sebastopol Cooperative Canning Company pose with "Apple Time" products that are boxed and ready to load on freight rail cars, circa 1968 clipping (Courtesy of the Western Sonoma County Historical Society)

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Figure 15 This 1970 photograph shows that original, circa 1924, fenestration and siding of the façade and side elevations of 6780 Sebastopol Avenue had been replaced with manufactured wood siding, known as T1-11 and fixed and sliding aluminum frame windows. Note that a contemporary sign of backlit plastic sign above the entrance reads "Sebastopol Co-op Cannery." (Courtesy of the online photographic archives collections of the Western Sonoma County Historical Society, accessed via the Sonoma County Library).

6782 Sebastopol Avenue evidences significant changes to fenestration, siding, and use in the latter half of the 20<sup>th</sup>-Century to the present. These alterations are commonly reflected in "Year Built" dates on parcel reports. 6782 Sebastopol Avenue's Parcel Report, Parcel #004-750-030-000 gives a Year Built of

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1953. The aluminum-frame windows and manufactured siding seen on 6782 Sebastopol Avenue in **Figure 14** may reflect 1953 alterations.

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### B10. Significance

The application of criteria to determine the eligibility of 6782 Sebastopol Avenue is conducted for the purpose of recommending whether the property is individually significant, as defined by CEQA guidelines. The four CRHR significance criteria closely mirror those of the NRHP and are routinely applied in tandem to one another to evaluate resources subject to CEQA and municipal compliance. The NRHP and CRHR recommendation in this study follow that tandem evaluation approach. NRHP criteria A, B, C & D and CRHR criteria 1, 2, 3 & 4 are jointly considered as Criterion A/1, referring to NRHP Criterion A and CRHR Criterion 1, as follows:

#### **NRHP Criterion A/CRHR Criterion 1 – *Not Recommend as Eligible***

To qualify for listing under Criterion A/1 of the NRHP/CRHR, a resource must be identified with an important event in history, an important pattern of events, or be significantly representative of a period of development. In review of historical documentation of 6782 Sebastopol, the former cannery railroad warehouse was not found to be mentioned in connection with a significant historical event. The warehouse was owned and operated by the Sebastopol Coop Cannery, earlier referred to as the Sebastopol Cooperative Canning Company, from which apple and other fruits were shipped as final products, the building is one of several apple, particularly Gravenstein apple, processing and product storage and shipping related structures at the eastern end of the City. No documentation of a known-eligible historic district is recorded here by the City of Sebastopol and the West Sonoma County Historic Resource Survey of 1981, revised in 2023, did not include the warehouse or neighboring warehouses worthy of inclusion or consideration. The aforementioned survey assessed and found historical resources within the blocks of the warehouse but did not include 6782 Sebastopol Avenue. The warehouse, built circa 1924/25, reflects Sebastopol's history of fruit packing and shipment by railroad but does not appear to be a significant representation of that period in the community's development. What associations the railroad warehouse building had with Sebastopol's renowned apple industry are overshadowed by the extant Gravenstein orchards, Luther Burbank experimental farm, and other character-defining structures and landscapes related to the development of the Gravenstein apple industry. Therefore, this resource is not recommended as eligible to the NRHP under Criterion A nor to the CRHR under Criterion 1.

#### **NRHP Criterion B/CRHR Criterion 2 – *Not Recommend as Eligible***

To qualify for listing under Criterion B/2 of the NRHP/CRHR, a resource must be identified with a person important in history. The founders of the Sebastopol Coop Cannery were Newton B. Kinley of Santa Rosa, Fred K. Hammell of Petaluma, Wendell Henderson of Kelseyville, and I. N. Cable, W. O. Barnes, James

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Blundin, and Henry Elphick of Sebastopol. These organizers and shareholders of the cannery do not appear to be persons of particular historical significance through their ownership relationship with the warehouse building. With particular attention to the four investors hailing from Sebastopol, namely Cable, Barnes, Blundin, and Elphick, the names did not appear in newspaper nor Western Sonoma County Historical Society articles or other online searches. Therefore, this resource is not recommended as eligible to the NRHP under Criterion B nor to the CRHR under Criterion 2.

### **NRHP Criterion C/CRHR Criterion 3 – *Not Recommend as Eligible***

To qualify for listing under Criterion C/3 of the NRHP/CRHR, a resource must be identified with important movements in, or masters of, design and construction or as representative of an historically significant architectural type. This resource, built circa 1924/25, is illustrative of a railroad warehouse type that was well established in the California by the 1920s and does not represent innovation or mastery in design or construction. Therefore, this resource is not recommended as eligible to the NRHP under Criterion C nor to the CRHR under Criterion 3.

### **NRHP Criterion D/CRHR Criterion 4– *Not Recommend as Eligible (Archaeological Resources not considered herein)***

To qualify for listing under Criterion D/4 of the NRHP/CRHR, a resource must have yielded or be likely to yield information important to prehistory or history. This study does not include the consideration of archaeological resources or of prehistory. However, as a built-environment or above-ground resource, the subject warehouse is a common structural form and does not embody information that may answer an unresolved historical question regarding design, construction, or history. Built forms of this type are well documented in current scholarship of building engineering and technology. Therefore, this resource is not recommended as eligible to the NRHP under Criterion D nor to the CRHR under Criterion 4.

### **Period of Significance**

Only properties that are recommended as individually eligible under at least one of the above criteria are historically significant and, therefore, capable of having a period of significance. Individually, this property, consisting of a parcel with a railroad warehouse, is not recommended as eligible to the NRHP or CRHR. Although the warehouse was built after the Sebastopol Coop Cannery Company's organization in 1924 and constructed soon thereafter, the property is recommended as ineligible under any NRHP and CRHR criteria in the section above and, therefore, has no historical significance that can be associated with a certain historical period.

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### Historical Integrity Assessment

The Department of Interior, National Park Service recognizes seven aspects of historical integrity, that of location, setting, design, workmanship, materials, feeling, and association. Only NRHP- or CRHR-eligible properties have features that require retention to convey that significance. 6782 Sebastopol Avenue is not recommended as significant against NRHP and CRHR criteria A/1, B/2, C/3, or, excluding archaeological evaluation, D/4. Therefore, the property does not possess character-defining features which would otherwise be associated with its significance.

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### B12. References:

California Public Law (website)

2023 CA Pub Res Code Section 5020.1 website  
[https://california.public.law/codes/ca\\_pub\\_res\\_code\\_section\\_5020.1](https://california.public.law/codes/ca_pub_res_code_section_5020.1), accessed 10/30/2023.

Fletcher, Francis.

1637 *"The world encompassed by Sir Francis Drake, being his next voyage to that to Nombre de Dios formerly imprinted; carefully collected out of the notes of Master Francis Fletcher, preacher in this employment, and diuers others his followers in the same; offered now at last to publique view ..."* [sic] *Sir Francis Drake.*

Hansen, Harvey J., Jeanne Thurlow Miller, and David Wayne Peri

1962 *Wild Oats in Eden: Sonoma County in the 19<sup>th</sup> Century.* Self-published, Kenwood, California.

Johnson, P.

1981 *Site Record for CA-SCR-187/H, P-44-000189.* Submitted to the Northwest Information Center.

Milliken, R., R.T. Fitzgerald, M. Hylkema, R. Groza, T. Origer, D.G. Bieling, A. Leventhal, R. Wiberg, A. Gottsfield, D. Gillette, V. Bellifemine, E. Strother, R. Cartier, and D.A. Fredrickson

2007 Punctuated Culture Change in the San Francisco Bay Area. In *California Prehistory: Colonization, Culture, and Complexity*, edited by T.L. Jones, and K.A. Klar, pp. 99-123. Altamira Press, New York.

Morgan, C



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1998 *Department of Parks and Recreation Primary Record for P-44-000377. On file at the Northwest Information Center, Rohnert Park, California.*

Sebastopol, City of (emails)

2023 Yarbrough Architectural Resources to , Senior Planner, Planning and Community Development Department. November 3, 2023.

Sonoma County (website)

2023 Laguna de Santa Rosa page at sonomacounty.com. Accessed November 27, 2023.

Western Sonoma County Historical Society

2003 *Sebastopol*. Images of America series, Acadia Publishing, San Francisco, California.

2011 *Sebastopol's Gravenstein Apple Industry*. Images of America series. Acadia Publishing, San Francisco, California.

Yarbrough Architectural Resources

2023 *Cultural Landscape Report: At Crossover Road, Olema Bolinas Road & California State Route 1*. Bolinas Lagoon Wye Wetlands Resiliency Project, Marin County Parks, Marin County, California. November 2023.

State of California  The Resources Agency  
 DEPARTMENT OF PARKS AND RECREATION  
**PRIMARY RECORD**

Primary #  
 HRI #  
 Trinomial  
**NRHP Status Code**

Other  
 Review Code

Reviewer

Date

Listings

Page 1 of 20 \*Resource Name or #: 6782 Sebastopol Avenue, Sebastopol, CA

P1. Other Identifier: Former Sebastopol Coop Cannery Company Warehouse

\*P2. Location:  Not for Publication  Unrestricted

\*a. County Sonoma

\*b. USGS 7.5' Quad Sebastopol Date 2018-09-05 T 11S; R 1W;  of  of Sec 18; B.M.

c. Address 6782 Depot St City Sebastopol Zip 95472

d. UTM: Zone 38.40318619701677, -122.82120527627568 e. Other Locational Data: Assessor's Parcel Number (APN): 004-750-033-000

\*P3a. Description: 6780 Depot Street is a utilitarian warehouse that appears to have been a box making and storage building for the Enterprise Bottling Company. (See DPR 523I continuation sheet page 3)

\*P3b. Resource Attributes: Industrial Building HP31.

\*P4. Resources Present:  Building  
 Structure  Object  Site  District  Element of District  Other

P5b. Description of Photo: View to NE, 3 Nov 2023, photo by Yarbrough Architectural Resources

\*P6. Date Constructed/Age and Source:  Historic  
1903 - 1911, Sanborn Fire Insurance Maps

\*P7. Owner and Address:

Barlow Star LLC  
6780 Depot Street, Suite 7  
Sebastopol, CA 95472

\*P8. Recorded by: Edward Yarbrough, Yarbrough Architectural Resources, 2150 Silverado Trl N, Saint Helena, CA 94574

\*P9. Date Recorded: Jan. 14, 2024

\*P10. Survey Type: (Describe)  
 Individual property  
 Historical Resource  
 Evaluation Report for Constraints Analysis

P5a. Photograph or Drawing



\*P11. Report Citation: (Cite survey report and other sources, or enter "none.")

HISTORICAL RESOURCE EVALUATION REPORT: Constraints Analysis for 6780 Depot St. & 6782 Sebastopol Ave., Sebastopol, Sonoma County, California Yarbrough Architectural Resources, January 2024

\*Attachments:  NONE  Location Map  Continuation Sheet  Building, Structure, and Object Record

Archaeological Record  District Record  Linear Feature Record  Milling Station Record  Rock Art Record

Artifact Record  Photograph Record  Other (List): \_\_\_\_\_

\*Resource Name or # (Assigned by recorder) \_\_\_\_\_ \*NRHP Status Code 6Z  
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B1. Historic Name: Enterprise Bottling Works Warehouse Building  
 B2. Common Name: 6780 Depot Street  
 B3. Original Use: Railroad Warehouse B4. Present Use: Commercial Rental Property \*B5.  
 Architectural Style: Industrial Railroad Warehouse circa 1903 - 1911  
 \*B6. Construction History: (Construction date, alterations, and date of alterations)

(See DPR 5231 Continuation Sheet beginning at page 14 of this form set)

\*B7. Moved?  No  Yes  Unknown

\*B8. Related Features: The building is part of a 12-acre district of adaptively reused industrial and railroad buildings called The Barlow.

B9a. Architect: unknown b. Builder: unknown  
 \*B10. Significance: Theme None Area The Barlow retail and commercial district  
 Period of Significance None Property Type Industrial Building HP31 Applicable Criteria None

(See DPR 5231 Continuation Sheet beginning at page 18 of this form set)

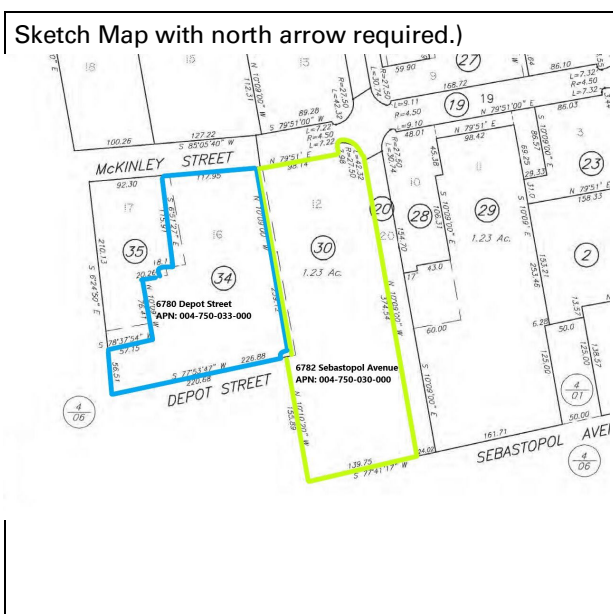
B11. Additional Resource Attributes: (List attributes and codes) None

\*B12. References:  
 (See DPR 5231 Continuation Sheet beginning at page 20 of this form set)

B13. Remarks:  
 Edward Yarbrough, a qualified architectural historian, conducted a site visit with photographs and notes on November 3, 2023 and research at the Sonoma County Recorder's Office, Sebastopol Public Library, Western Sonoma County Historical Society, as well as numerous other online sources of information.

\*B14. Evaluator: Edward Yarbrough, Architectural Historian & Principal, Yarbrough Architectural Resources  
 \*Date of Evaluation: January 2024

(This space reserved for official comments.)



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### P3a. Description

#### Parcel

- 6780 Depot Street-
  - APN: 004-750-033-000
  - Partition of Birdie Miller Cnopius
  - Recorded May 7, 1918 in Book 35, Maps pages 22-00
  - Lot 34
  - 0.9-acres
  - Road frontage of 226.88-feet and irregular depth of 239.12-feet.
  - Latitude: 38.4033; Longitude: -122.8213

#### Building

6780 Depot Street is a utilitarian warehouse that appears to have been a box making and storage building for the Enterprise Bottling Works. The greatly altered building now has a newer raised concrete foundation, although the 1911 Sanborn Fire Insurance Map indicates that the building was originally on a wood foundation of some type. The wood frame building is composed of two primary interior spaces, a 2-story office portion at the south end, that was built for box making, and a single-story storage building portion that extends back to McKinley Street. Therefore, this building is described here in two parts, the South 2-Story Unit and the North Unit.

#### *South 2-Story Unit*

The south 2-story unit of 6780 Depot Street is a side-gable mass with parapet walls at each end. They feature a two-step parapet wall with a central, ridge rectangular step. The façade does not appear to retain any materials from its period of construction in the first decade of the 20<sup>th</sup>-Century. The building is clad in what appears to be a bitumen layer held in place by battens, spaced approximately 2-feet apart vertically. A concrete front porch with low-slope shed roof and concrete porch appears to date from approximately the last 40-years. The fenestration on the façade, as well as on the façade-unit of the building, are dark bronzed, aluminum-frame horizontal sliders. Of similar date, an exterior stairway with a landing and door to the second floor and contemporary office doors and windows on the first floor give the building a contemporary appearance.

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Figure 1 (Photography by Yarbrough Architectural Resources 11/3/2023).

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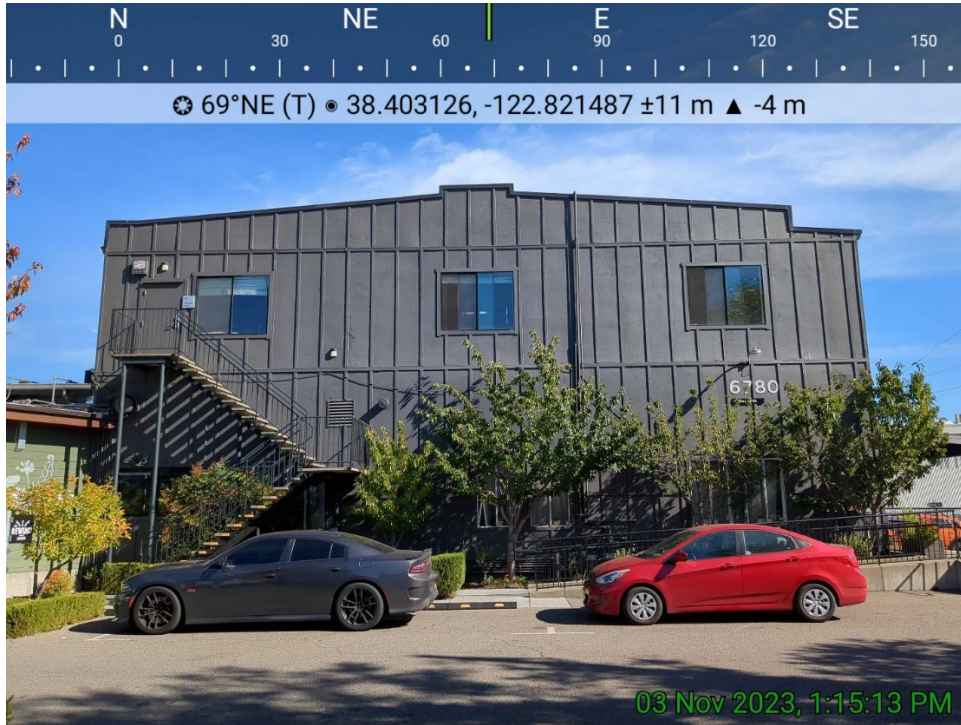


Figure 2 (Photography by Yarbrough Architectural Resources 11/3/2023).

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Figure 3 (Photography by Yarbrough Architectural Resources 11/3/2023).

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Figure 4 (Photography by Yarbrough Architectural Resources 11/3/2023).



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Figure 5 (Photography by Yarbrough Architectural Resources 11/3/2023).

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Figure 6 (Photography by Yarbrough Architectural Resources 11/3/2023).

### North Unit

The north unit of the building is single-story parapet, gable-front building with side eaves. It is skirted with broad porches on its east, north, and west elevations. Built for storage, the roof and porch roofs are high to accommodate the movement of boxed bottles for loading on railroad freight cars on the east side of the building unit and for truck access on the west elevation. The plan is rectangular with a pushed-out addition on the east elevation at the northeast corner and another on the west elevation at the southwest corner

A secondary façade facing McKinley Street, the north unit presents a gradually sloping parapet wall with a small raised rectangular step at the ridge and flattened corner casts that appear to be a design feature remaining from its period of construction. Between this secondary façade and McKinley Street, the building had a contemporary concrete terrace with seating and modern railing and access features.

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Figure 7 View to the southeast (Photography by Yarbrough Architectural Resources 11/3/2023).

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Figure 8 Note visible solar panels from McKinley Street (Photography by Yarbrough Architectural Resources 11/3/2023).

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Figure 9 (Photography by Yarbrough Architectural Resources 11/3/2023)

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Figure 10 (Photography by Yarbrough Architectural Resources 11/3/2023)

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**B6. Construction History:** (Construction date, alterations, and date of alterations)

At the time of the 1911 Sanborn Insurance Map both sides of the block where 6780 Depot Street sits included 5-residences, a Roman Catholic Church, and the H. H. Laton finished lumber warehouse and lumber yard.

The fruit processing and canning buildings and railroad spurs that connected them to distant markets were built west of the Laguna de Santa Rosa wetlands between 1903 and 1911, according to Sanborn Fire Insurance maps from those dates. The first railroad warehouses recorded in the area by 1911 were the Italian-Swiss Colony Winery & Distillery, the Central California Canneries Company, and a box-making and warehouse that may have been the Enterprise Bottling Works at 6780 Depot Street. These structures were connected to railroad track spurs from the Western Pacific Railroad line, a company formed in 1903 to compete with the Southern Pacific Railroad monopoly (Western Sonoma County Historical Society 2003).

6780 Depot Street primary unit, now the 2-story façade with front porch, is labeled as a space for “box making” and had a partial deck along the eastern portion of the façade. The front portion is labeled as having wood or post foundations while the rear extension that runs back to McKinley Street had a concrete perimeter wall.

In the center of the 1965 aerial photograph (**Figure 12**), 6780 Depot Street is the building directly above the elongated form of 6782 Sebastopol Avenue. Note that in both this aerial photograph and the view of McKinley Street in **Figure 11**, that an overhead bridge is visible. This may have been a part of an overhead conveyer belt system. Its presence and a shared address number suggests that 6780 Depot Street was historically connected to operations across the street and to its rear at 6780 McKinley Street, a property clearly labeled Enterprise Bottling Works in the 1911 Sanborn Fire Insurance Map.

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*Figure 11 1951 at McKinley Street, which runs east-west behind both industrial structures and through the middle of the site. The building with light cladding and gable roof to the left of the cars is 6782 Sebastopol Avenue and the darker building with a hip roof and a conveyor bridge over the street is the real portion of 6780 Depot Street (Courtesy of the online photographic archives collections of the Western Sonoma County Historical Society, accessed via the Sonoma County Library).*

Railroad spurs and access to main lines were increasingly abandoned in the County as products were shipped by truck, a trend that accelerated following World War II.



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*Figure 12 Aerial photo taken from the east of the subject properties in 1965 (Courtesy of the online photographic archives collections of the Western Sonoma County Historical Society, accessed via the Sonoma County Library).*

6780 Depot Street evidences significant changes to fenestration, siding, and use in the latter half of the 20<sup>th</sup>-Century to the present. These alterations are commonly reflected in “Year Built” dates on parcel reports. 6780 Depot Street’s Parcel Report, Parcel #004-750-034-000 provides a Year Built of 1965. 6780 Depot Street shows many alterations to the primary mass facing Depot Street, including late 20<sup>th</sup> or even early 21<sup>st</sup>-Century bronzed aluminum sliding tinted windows and exterior mounted concrete and steel stairways with switchback landings, and a shed-roof porch with side wheelchair ramp on contemporary concrete footings.

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### B10. Significance

#### Application of the Significance Criteria to 6780 Depot Street

The application of criteria to determine the eligibility of 6780 Depot Street is conducted for the purpose of recommending whether the property is individually significant, as defined by CEQA guidelines. The four CRHR significance criteria closely mirror those of the NRHP and are routinely applied in tandem to one another to evaluate resources subject to CEQA and municipal compliance. The NRHP and CRHR recommendation in this study follow that tandem evaluation approach. NRHP criteria A, B, C & D and CRHR criteria 1, 2, 3 & 4 are jointly considered as Criterion A/1, referring to NRHP Criterion A and CRHR Criterion 1, as follows:

#### NRHP Criterion A/CRHR Criterion 1 – *Not Recommend as Eligible*

To qualify for listing under Criterion A/1 of the NRHP/CRHR, a resource must be identified with an important event in history. In review of historical documentation of Sebastopol's development and the area where cannery warehouses are present adjacent to rail and road networks, 6780 Depot Street was not found to be mentioned in connection with a significant historical event. Although the extant warehouse was constructed between 1905 and 1911, as shown on Sanborn Fire Insurance maps of those dates, the building is part of a pattern of industrial development in the area that began at the turn of the 20<sup>th</sup> Century and continued into the 1970s. The property was subject to historical inventory and review, as recorded in the *Western Sonoma County Historic Resources Survey, Volume 2. The City of Sebastopol* (1981; reprinted and revised 2023) and specifically excluded from the list of historical resources listed therein. This Historic Resource Survey included properties with significance due to association with important historical events and associations. The warehouse building and its property are not associated with an important historical event or exceptionally representative of the City's industrial or transportation development. Therefore, this resource is not recommended as eligible to the NRHP under Criterion A nor to the CRHR under Criterion 1.

#### NRHP Criterion B/CRHR Criterion 2 – *Not Recommend as Eligible*

To qualify for listing under Criterion B/2 of the NRHP/CRHR, a resource must be identified with a person important in history. No individual of historical significance in the history of Sebastopol could be identified with the subject property. The property was considered in the historical inventory and review, as recorded in the *Western Sonoma County Historic Resources Survey, Volume 2. The City of Sebastopol* (1981; reprinted and revised 2023) and specifically excluded from the list of historical resources listed therein. This Historic Resource Survey included properties with significance due to association with an important individual in history. The warehouse building and its property are not associated with an important individual associated with the City's development. Therefore, this resource is not recommended as eligible to the NRHP under Criterion B nor to the CRHR under Criterion 2.

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### NRHP Criterion C/CRHR Criterion 3 – *Not Recommend as Eligible*

To qualify for listing under Criterion C/3 of the NRHP/CRHR, a resource must be identified with important movements in, or masters of, design and construction or as representative of an historically significant architectural type. This resource is illustrative of a railroad warehouse type that was well established in the California by the 1920s and does not represent innovation or mastery in design or construction. Therefore, this resource is not recommended as eligible to the NRHP under Criterion C nor to the CRHR under Criterion 3.

### NRHP Criterion D/CRHR Criterion 4– *Not Recommend as Eligible* (Archaeological Resources not considered herein)

To qualify for listing under Criterion D/4 of the NRHP/CRHR, a resource must have yielded or be likely to yield information important to prehistory or history. This study does not include the consideration of archaeological resources or of prehistory. However, as a built-environment or above-ground resource, the subject warehouse is a common structural form and does not embody information that may answer an unresolved historical question regarding design, construction, or history. Built forms of this type are well documented in current scholarship of building engineering and technology. Therefore, this resource is not recommended as eligible to the NRHP under Criterion D nor to the CRHR under Criterion 4.

### Period of Significance

Only properties that are recommended as individually eligible under at least one of the above criteria are historically significant and, therefore, capable of having a period of significance. Individually, this property, consisting of a parcel with a railroad warehouse, is not recommended as eligible to the NRHP or CRHR. Although the warehouse was built sometime in the first decade of the 20<sup>th</sup>-Century, the property is recommended as ineligible under any NRHP and CRHR criteria in the section above and, therefore, has no historical significance that can be associated with a certain historical period.

### Historical Integrity Assessment

The Department of Interior, National Park Service recognizes seven aspects of historical integrity, that of location, setting, design, workmanship, materials, feeling, and association. Only NRHP- or CRHR-eligible properties have features that require retention to convey that significance. 6780 Depot Street is not recommended as significant against NRHP and CRHR criteria A/1, B/2, C/3, or, excluding archaeological evaluation, D/4. Therefore, the property does not possess character-defining features which would otherwise be associated with its significance.

## CONTINUATION SHEET

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### B12. References:

California Public Law (website)

2023 CA Pub Res Code Section 5020.1 website  
[https://california.public.law/codes/ca\\_pub\\_res\\_code\\_section\\_5020.1](https://california.public.law/codes/ca_pub_res_code_section_5020.1), accessed 10/30/2023.

Fletcher, Francis.

1637 *"The world encompassed by Sir Francis Drake, being his next voyage to that to Nombre de Dios formerly imprinted; carefully collected out of the notes of Master Francis Fletcher, preacher in this employment, and diuers others his followers in the same; offered now at last to publique view ..." [sic] Sir Francis Drake.*

Hansen, Harvey J., Jeanne Thurlow Miller, and David Wayne Peri

1962 *Wild Oats in Eden: Sonoma County in the 19<sup>th</sup> Century.* Self-published, Kenwood, California.

Johnson, P.

1981 *Site Record for CA-SCR-187/H, P-44-000189.* Submitted to the Northwest Information Center.

Milliken, R., R.T. Fitzgerald, M. Hylkema, R. Groza, T. Origer, D.G. Bieling, A. Leventhal, R. Wiberg, A. Gottsfield, D. Gillette, V. Bellifemine, E. Strother, R. Cartier, and D.A. Fredrickson

2007 Punctuated Culture Change in the San Francisco Bay Area. In *California Prehistory: Colonization, Culture, and Complexity*, edited by T.L. Jones, and K.A. Klar, pp. 99-123. Altamira Press, New York.

Morgan, C

1998 *Department of Parks and Recreation Primary Record for P-44-000377. On file at the Northwest Information Center, Rohnert Park, California.*

## CONTINUATION SHEET

Property Name: \_\_\_\_\_

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### Sebastopol, City of (emails)

2023 Yarbrough Architectural Resources to , Senior Planner, Planning and Community Development Department. November 3, 2023.

### Sonoma County (website)

2023 Laguna de Santa Rosa page at sonomacounty.com. Accessed November 27, 2023.

### Western Sonoma County Historical Society

2003 *Sebastopol*. Images of America series, Acadia Publishing, San Francisco, California.

2011 *Sebastopol's Gravenstein Apple Industry*. Images of America series. Acadia Publishing, San Francisco, California.

### Yarbrough Architectural Resources

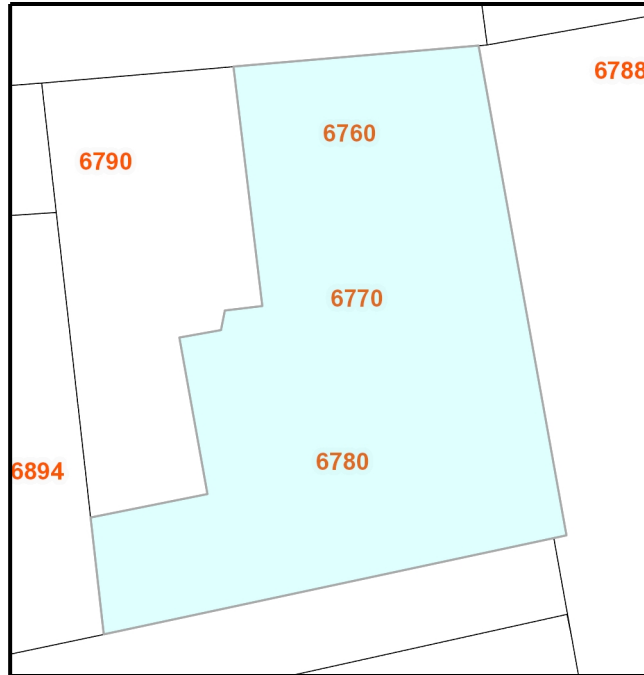
2023 *Cultural Landscape Report: At Crossover Road, Olema Bolinas Road & California State Route 1*. Bolinas Lagoon Wye Wetlands Resiliency Project, Marin County Parks, Marin County, California. November 2023.

## Appendix B: Property and Ownership Records

County Assessor Information

12/1/2023 11:25:03 AM

Situs Address: 6780 DEPOT ST  
Situs CSZ: SEBASTOPOL, CA 95472



Land Use: MISC MULTIPLE USE/NO DOMI

Tax Area: 005014

Jurisdiction: SEBASTOPOL

Recording#: 2016R104809

Rec Date: 11/10/2016

Land Value: \$1,032,295

Improvements: \$1,339,130

Owner Exempt: \$0

Subdivision:

Commercial Units:

Residential Units:

Bedrooms:

Bathrooms:

Main Bldg SqFt: 23,563

Year Built: 1965

Lot Acres: 0.90

GIS Calculated Information

Local Information

Lot Acres: 0.90

Zoning Code: CM

Census Tract: 153406

General Plan Code: LI

Census Block: 1015

Latitude: 38.4033

Longitude: -122.8213

This report is a user generated static output from an Internet mapping site and is for reference only. Data that appear on this report may or may not be accurate, current, or otherwise reliable. GIS Calculated Lot Acres is NOT official. Assessor Data is maintained by Sonoma County.

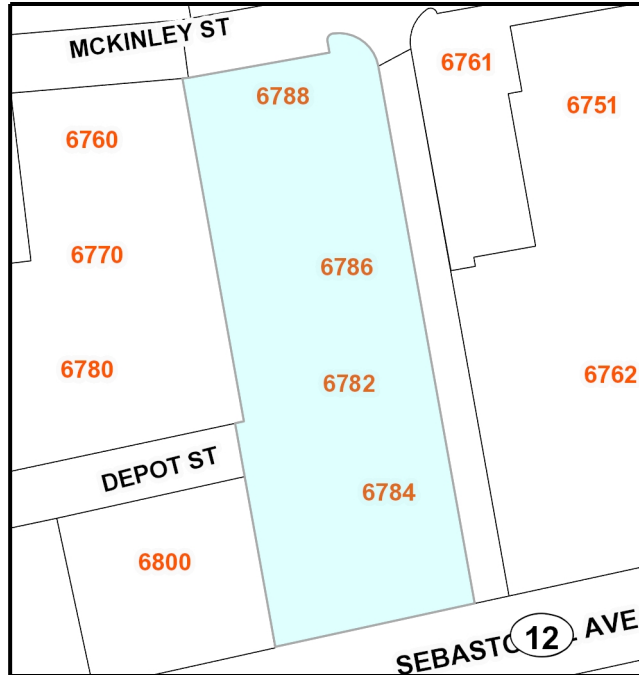
# Parcel Report

Parcel #: 004-750-030

## County Assessor Information

12/1/2023 11:23:08 AM

**Situs Address:** 6782 SEBASTOPOL AVE  
**Situs CSZ:** SEBASTOPOL, CA 95472



**Land Use:** MISC MULTIPLE USE/NO DOMI

**Tax Area:** 005014

**Jurisdiction:** SEBASTOPOL

**Recording#:** 2016R104818

**Rec Date:** 11/10/2016

**Land Value:** \$1,409,293

**Improvements:** \$1,830,819

**Owner Exempt:** \$0

**Subdivision:**

**Commercial Units:**

**Residential Units:**

**Bedrooms:**

**Bathrooms:**

**Main Bldg SqFt:** 31,571

**Year Built:** 1953

**Lot Acres:** 1.23

## GIS Calculated Information

## Local Information

**Lot Acres:** 1.23

**Zoning Code:** CM

**Census Tract:** 153406

**General Plan Code:** LI

**Census Block:** 1015

**Latitude:** 38.4032

**Longitude:** -122.8207



# COUNTY ASSESSOR'S PARCEL MAP

TAX RATE AREA  
5-014

004-75

Ptn of BIRDIE MILLER CNOPIUS  
REC. 08-28-1918 IN BK. 35, MAPS, PGS. 22-00

Record of Survey  
REC. 04-07-2011 IN BK. 745, MAPS, PGS. 10-11

Record of Survey  
REC. 05-28-2013 IN BK. 757, MAPS, PGS. 43/44



SCALE: 1"=100'

REVISED  
08-06-12=21-KB  
09-17-12=22-KB  
10-24-12=36-KB  
11-20-13=R/S-KE

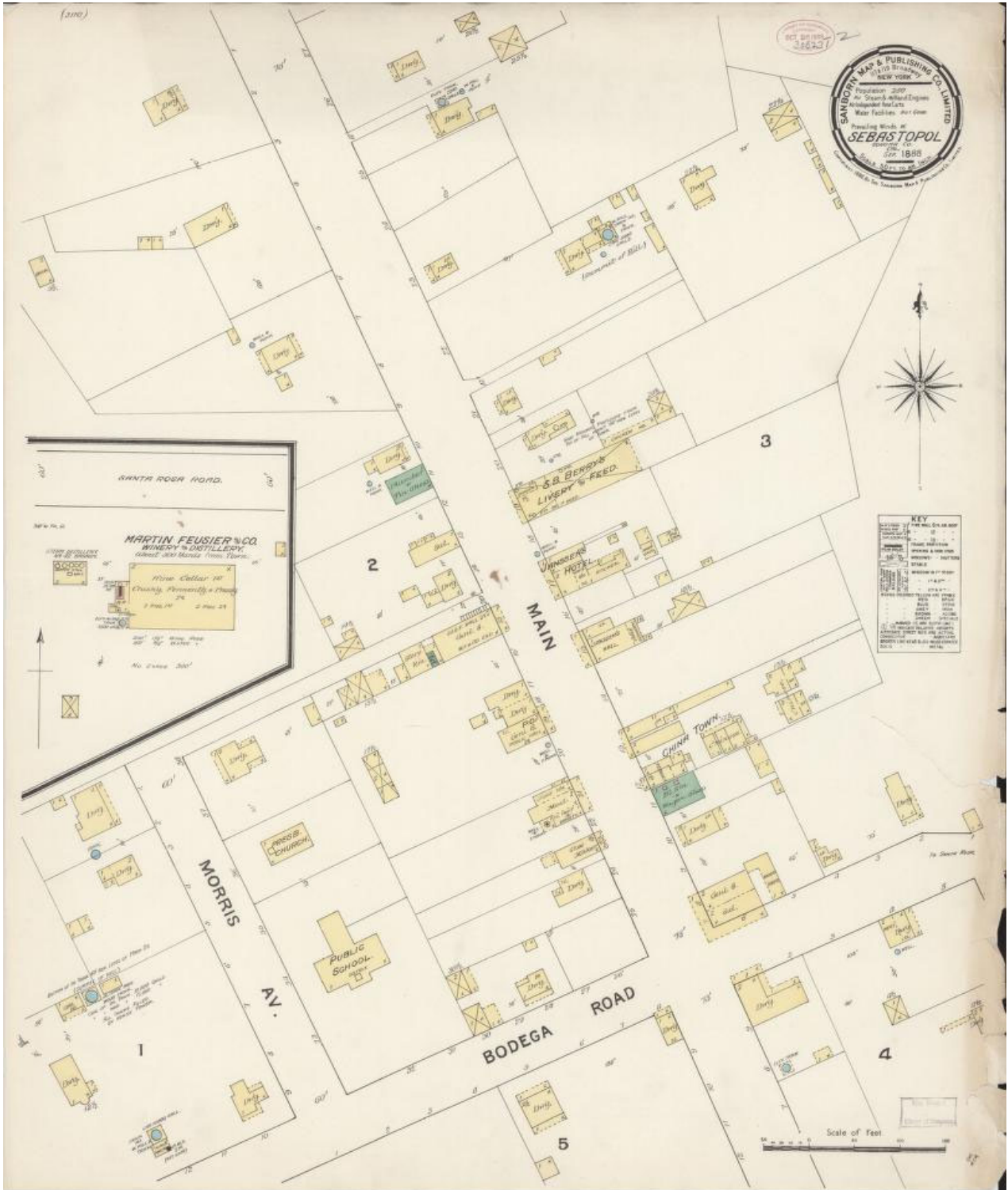
NOTE: This map was prepared for Assessment purposes only and does not indicate either parcel legality or a valid building site. No liability is assumed for the accuracy of the data delineated. The acreages are based on the information supplied to the Assessor (i.e. recorded survey maps, recorded deeds, prior assessment maps, etc.)

NOTE: Assessor's parcels do not necessarily constitute legal lots. To verify legal parcel status, check with the appropriate city or county community development or planning division.

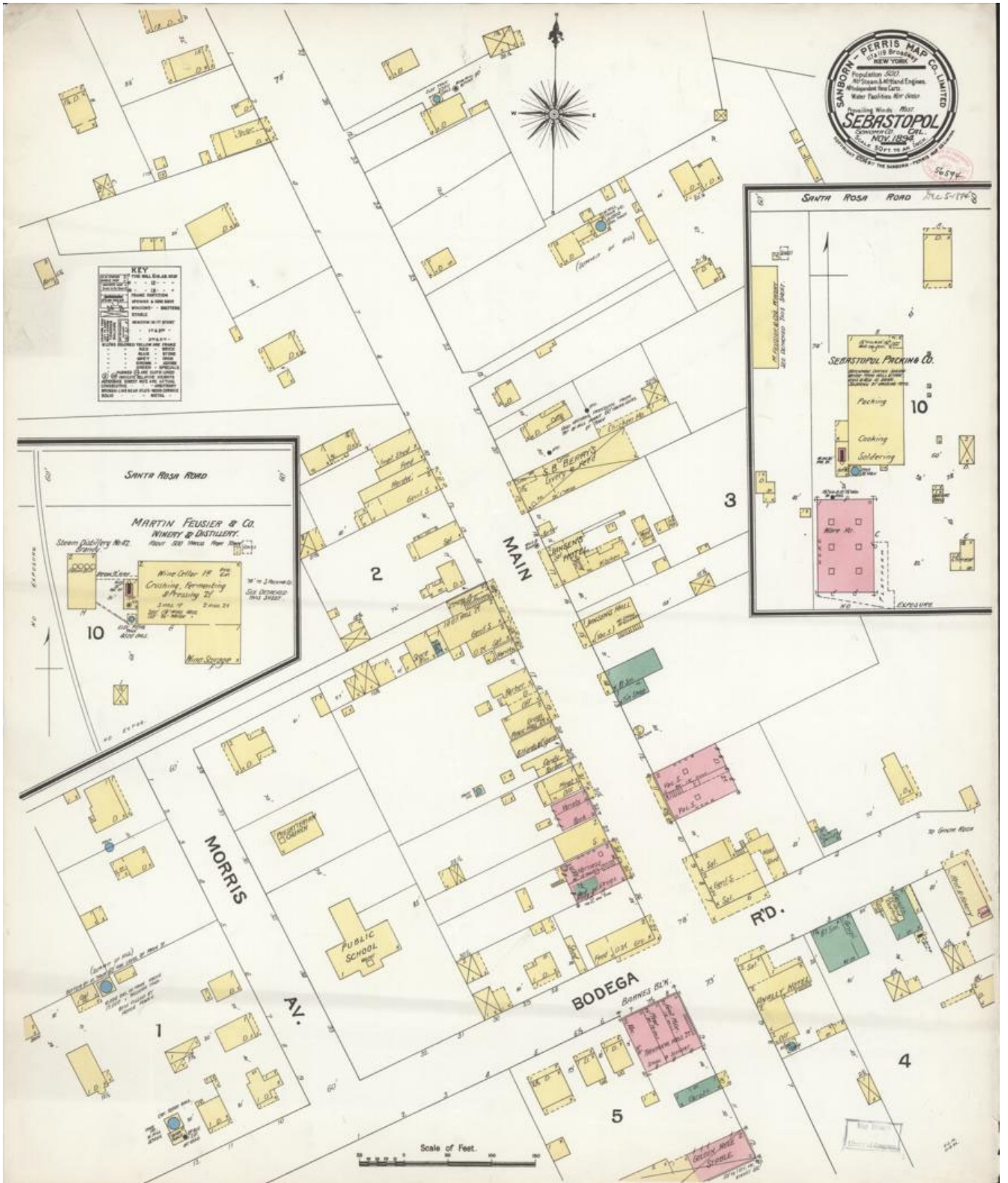
Assessor's Map Bk. 004, Pg. 75  
Sonoma County, Calif. (ACAD)

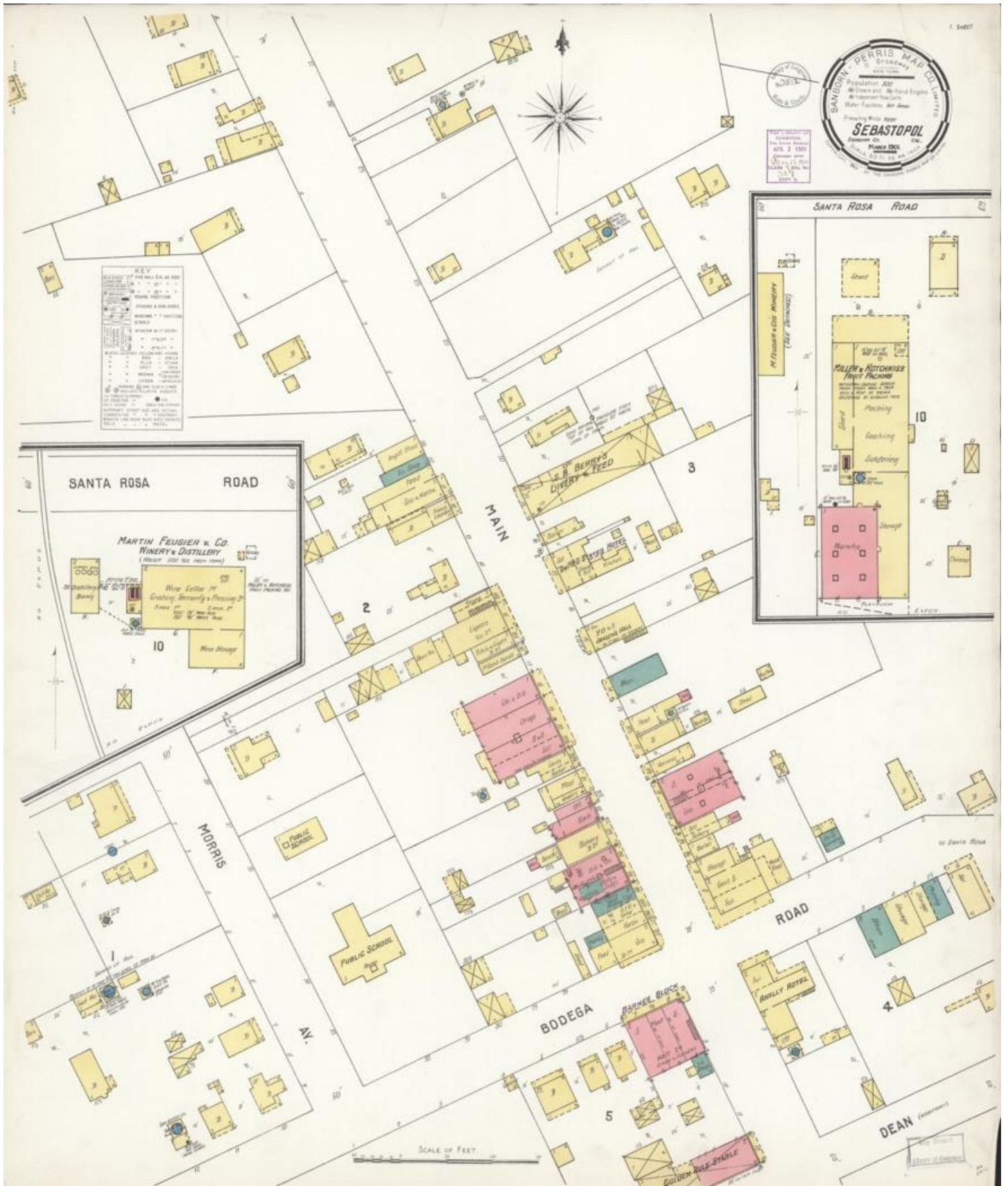
KEY 8-6-12=21 KB

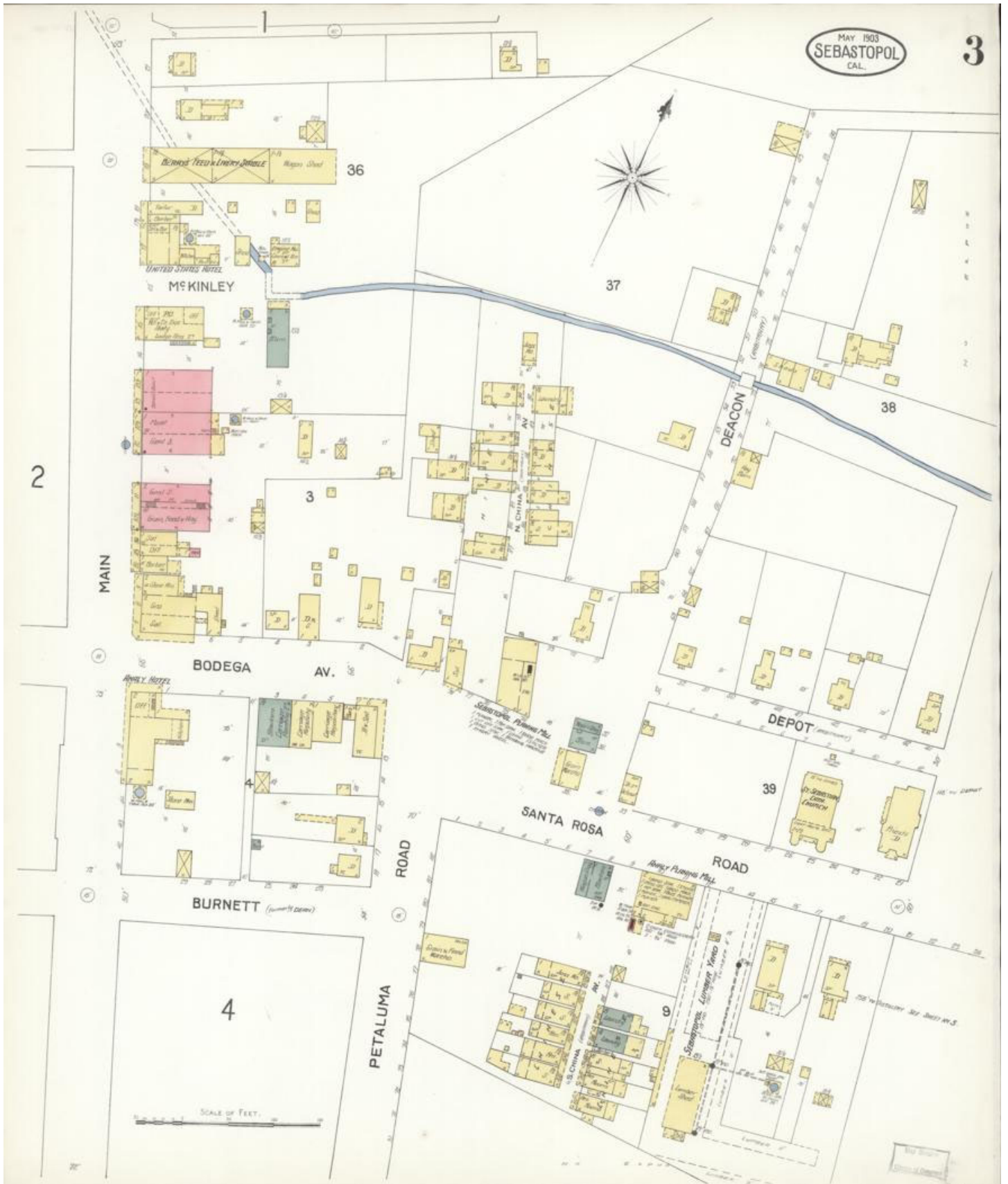
## Appendix C: Sanborn Fire Insurance and Plat Maps and Aerial Photographs





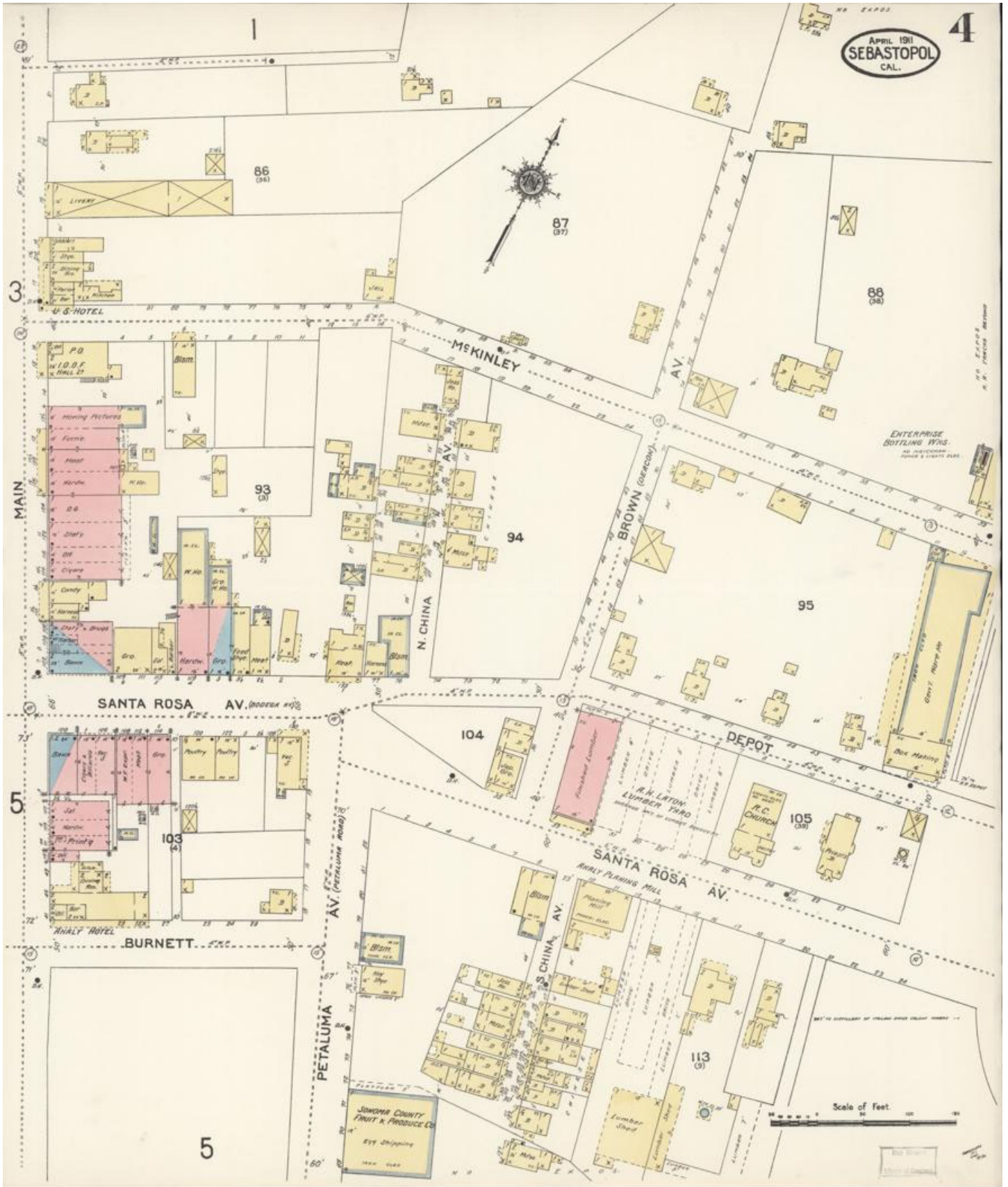






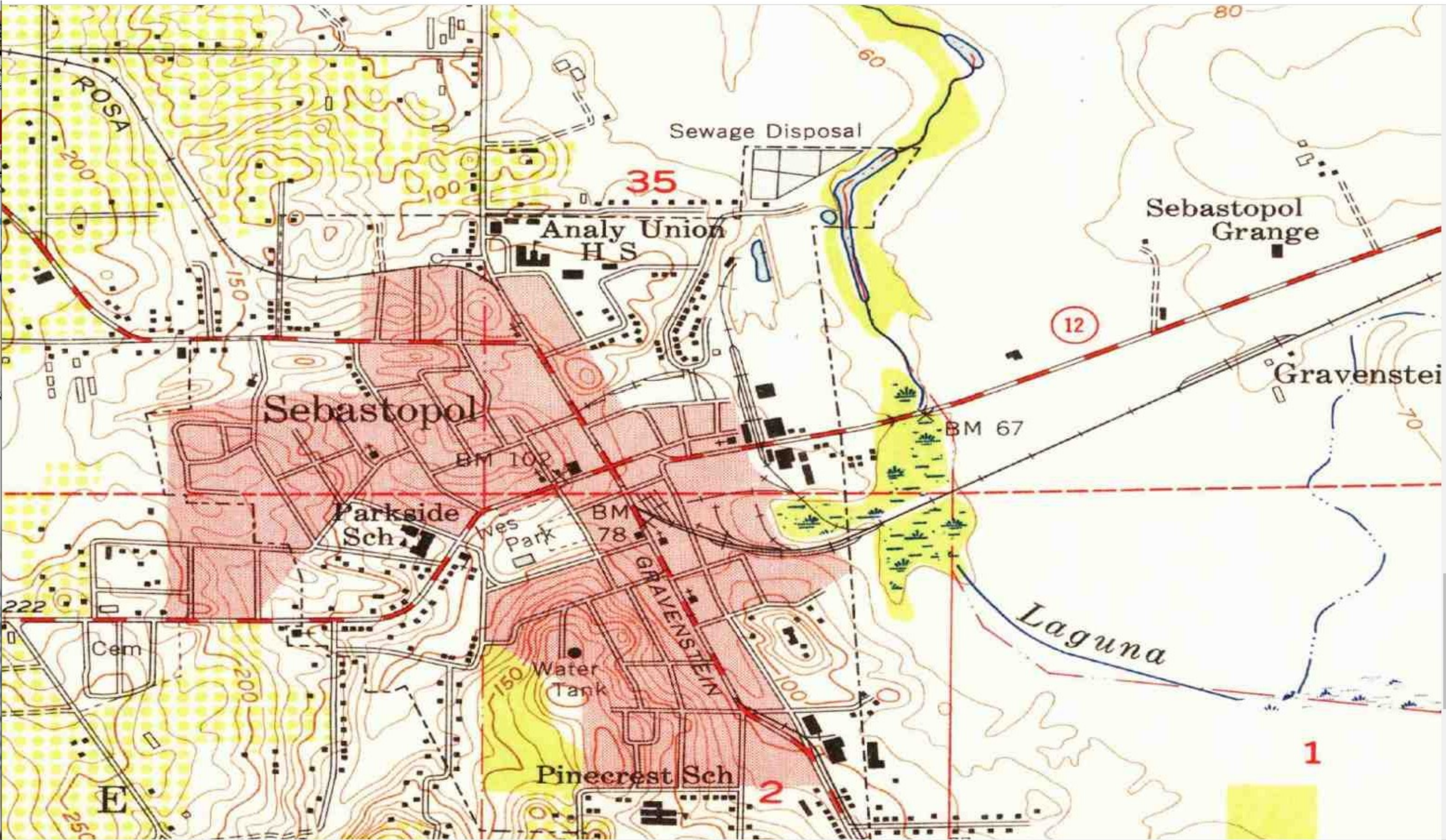










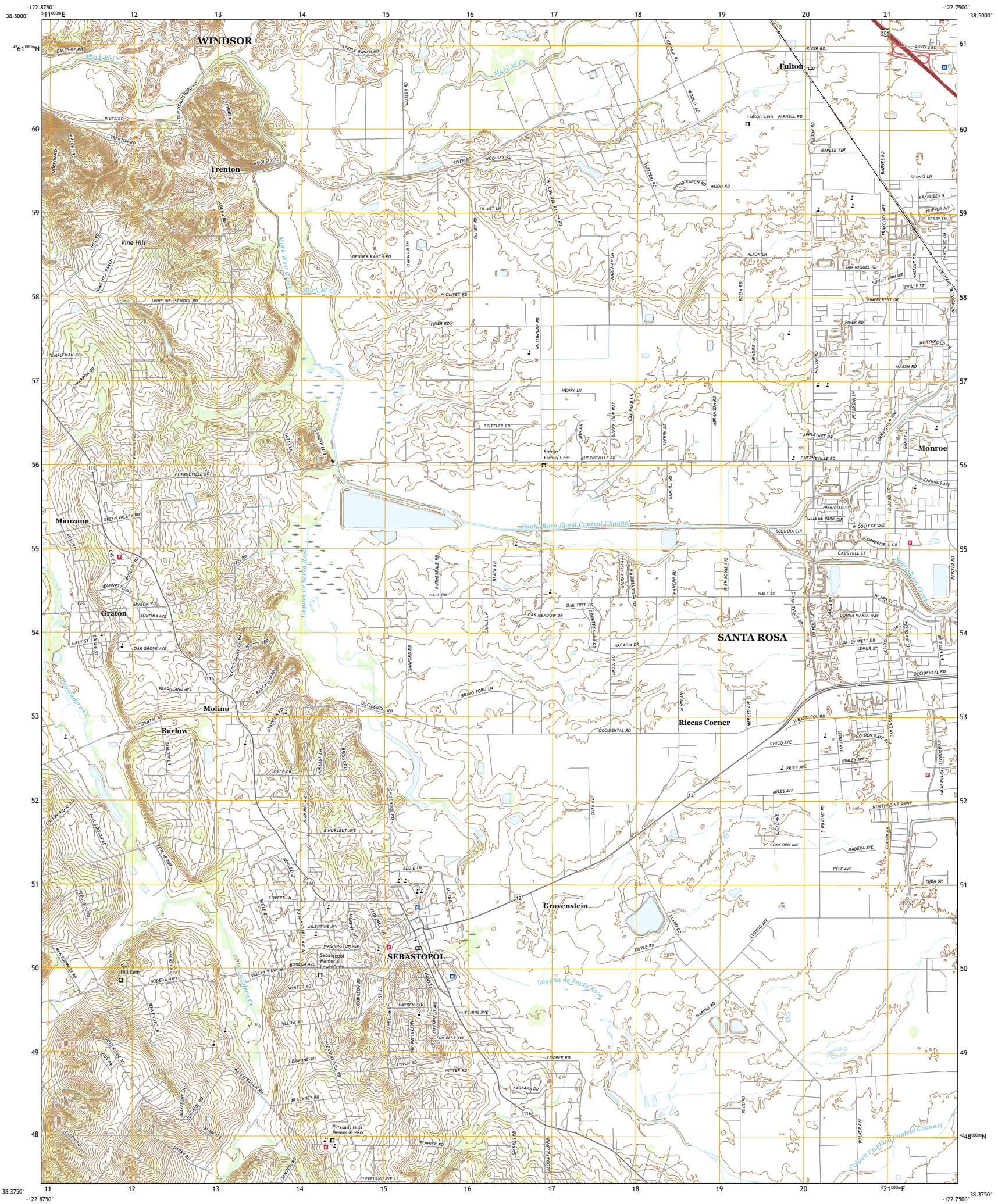




U.S. DEPARTMENT OF THE INTERIOR  
U.S. GEOLOGICAL SURVEY

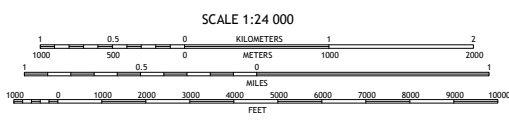
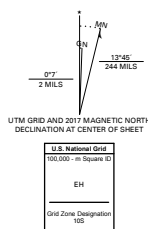


SEBASTOPOL QUADRANGLE  
CALIFORNIA - SONOMA COUNTY  
7.5-MINUTE SERIES



**Produced by the United States Geological Survey**  
North American Datum of 1983 (NAD83);  
World Geodetic System of 1984 (WGS84). Projection and  
1 000 meter grid: Universal Transverse Mercator, Zone 10S  
This map is not a legal document. Boundaries may be  
generalized for this map scale. Private lands within government  
reservations may not be shown. Obtain permission before  
entering private lands.

Imagery:.....NAP, June 2016 - October 2016  
Roads:.....U.S. Census Bureau, 2016  
Names:.....GNIS, 1981 - 2016  
Hydrography:.....National Hydrography Dataset, 2003  
Contours:.....National Elevation Dataset, 2017  
Boundaries:.....Multiple sources; see metadata file 2016 - 2017  
Public Land Survey System:.....BLM, 2018  
Wetlands:.....FWS National Wetlands Inventory 1972 - 1985



1	2	3
4	5	6
7	8	

ADJOINING QUADRANGLES

**ROAD CLASSIFICATION**

	Expressway		Local Connector
	Secondary Hwy		Local Road
	Ramp		4WD
	Interstate Route		US Route
			State Route

SEBASTOPOL, CA  
2018





