


Agenda Report Reviewed by:
City Manager: 

CITY OF SEBASTOPOL
CITY COUNCIL
AGENDA ITEM

Meeting Date: February 15, 2022
To: Honorable Mayor and City Councilmembers
From: Larry McLaughlin, City Manager
Mario Landeros, Interim City Engineer
Subject: Parklets in Caltrans Right-of-Way
Recommendation: Provide Direction to Staff regarding the Parklets
Funding: Currently Budgeted: _____ Yes _____ No X N/A

Account Code/Costs authorized in City Approved Budget (if applicable) AK (verified by Administrative Services Department)

INTRODUCTION/PURPOSE:

The item is to inform the Council on the forthcoming Caltrans encroachment permit extension approval and to request that Council discuss and provide direction to staff regarding parklets on State right-of-way.

BACKGROUND AND DISCUSSION:

At their October 6, 2020 meeting, Council directed staff to submit a Caltrans encroachment permit application for parklets in the City’s name. The original encroachment permit issued by Caltrans for temporary parklets has an expiration date of February 28, 2022. In January 2022, in a discussion with the Interim City Engineer, Caltrans senior staff indicated that upon a written request made by the City an extension to the permit would be approved. Staff has submitted the required permit extension request to Caltrans to allow the temporary parklets for another year. A copy of the extension letter (Attachment 1) and the original permit (Attachment 2) are included in this report.

The locations of the three temporary parklets (“Parklets”) are:

1. In front of “Screamin’ Mimi’s/Create It/The Face Place/Thrive Yogo” on Sebastopol Avenue north side;
2. In front of “People’s Music/Sumbody” on N. Main Street, west side, the block between McKinley St. and Bodega Ave, south of the McKinley Crosswalk; and
3. In front of “Sunshine Café/Retrograde Coffee Roasters” on S. Main Street, west side, mid-block between Bodega Avenue and Burnett Street.

City staff did initial outreach, including surveys, phone calls, and a meeting in September 2020 with business owners along the three blocks where the Parklets were to be installed. The Parklets were installed shortly thereafter. In September 2021, City staff held another meeting with the business owners near the Sebastopol Avenue parklets to discuss any issues. At this meeting some issues were raised with this parklet given the unusual shape and how it impacts the access to some of these businesses (especially those who depend on pickup/drop off business during Covid and normal operations, given the lack of parking along Sebastopol Avenue except where parking was removed to install the parklet). City staff secured temporary use of two parking spaces in the West America parking lot as

'curbside pickup' short term parking to assist with this issue, however these are a temporary accommodation as a community-minded effort from the bank.

City staff also received comments of concern from business owners after the meeting regarding this parklet, as well as the second parklet listed above (in front of People's Music). Staff has not received any negative comments from business owners near the third (Sunshine Café/Retrograde) parklet, and in fact one of the business owners has inquired if it could be made permanent.

Additionally, the City conducted a public survey in May 2021 regarding the Parklets and the survey results are included as Attachment 3 to this report. At that time, out of 567 responses received approximately 80% of respondents wanted to see the Parklets continue.

Because of substantial community interest in the Parklets together with varied opinions of business owners and the public, and the impending expiration of the current encroachment permit at the end of this month, the Interim City Engineer requested a 1-year extension that will allow the temporary parklets to remain in Caltrans right-of-way. The extension will allow time for the Council to discuss the future of the parklets, including concerns and requests from some business owners and property owners.

GOALS:

This action supports the following City Council Goals and General Plan Actions:

- Goal 8 - Enhance and Maintain the Economic Vitality of the City

PUBLIC COMMENT:

As of the writing of this staff report, the City has received several comments, via email communication from affected merchants with various positions on the Parklet however just two of the comments were specifically asked to be included as public comments with this report, and are therefore included as attachments. Staff anticipates receiving additional public comment from interested parties following the publication and distribution of this staff report. Such comments will be provided to the City Council as supplemental materials before or at the meeting. In addition, public comments may be offered during the public comment portion of the agenda item.

PUBLIC NOTICE:

This item was noticed in accordance with the Ralph M. Brown Act and was available for public viewing and review at least 72 hours prior to schedule meeting date.

FISCAL IMPACT:

Depending on Council direction, there may be fiscal impact for future activities related to this item.

RECOMMENDATION:

Staff recommends the Sebastopol City Council discuss the continuing use of Parklets and address the following:

- Should the Parklets remain temporary at this time?
- Should one, two, or all three parklet locations continue to be permitted?
- Should staff pursue an encroachment permit for permanent Parklets and if so, for which of the parklet locations?

Furthermore, if Council is in consensus with pursuing permanent parklets, staff would recommend that all design questions pertinent to permanent Parklets be forwarded to the Planning Commission as the Parks Commission for its input and recommendations for the City Council's consideration at a future meeting.

Attachment:

- 1 Letter of Extension Request to Caltrans
- 2 Caltrans Encroachment Permit for Parklets
- 3 Temporary Parklet Survey Summary Results (May 2021)
- 4 Public Comments



Agenda Item Number 12
Engineering Department
714 Johnson Street
Sebastopol, CA 95472
Phone: (707) 823-2151
Fax: (707) 823-4721
Website: www.ci.sebastopol.ca.us
Email: engineering@cityofsebastopol.org
Mario Landeros, Interim City Engineer

January 27, 2022

Mr. Chris Master, Senior Permit Engineer
Caltrans District 4
Encroachment Permits
111 Grand Avenue, 6th Floor MS 5E
Oakland, CA 94623-0660

**RE: 04-20-N-MC-2398 City of Sebastopol Parklets
04-SON-116-26.733**

Dear Mr. Master:

This letter is to request amending the above listed encroachment permit (copy attached) for a one (1)-year extension, which is currently set to expire February 28, 2022.

During the term of the current encroachment permit, use of the temporary parklets by the general public has aided our community members and local businesses through many challenges due to the ongoing pandemic from COVID-19 and subsequent variants that continue plaguing the State. By approving our request for a 1-year extension Sebastopol may be able to continue enjoyment, convenience, and safety through use of the temporary parklets.

The City greatly appreciates in advance your favorable review and approval of this request before the current permit expires. If you have any questions or require additional information, please do not hesitate to contact me at your earliest convenience. I can be reach by phone (707 236-1542) or email (mario.landeros@ghd.com).

Thank you for your consideration.

Mario Landeros, Interim City Engineer

Cc: Lawrence McLaughlin, City Manager
Engineering File

Attachment(s): Permit No. 04-20-N-MC-2398
Rider No. 04-21-N-RT-0322

STATE OF CALIFORNIA • DEPARTMENT OF TRANSPORTATION

ENCROACHMENT PERMIT RIDER

TR-0122 (REV 6/1999)

Collected by	Permit No. (Original)
	04-20-N-MC-2398
Rider Fee Paid \$Exempt	Dist/Co/Rte/PM 04/SON/116/26.733
Date February 5, 2021	Rider Number 04-21-N-RT-0322

TO: [] City of Sebastopol []
 714 Johnson Street
 Sebastopol, CA 95472
 Email: j_gaffney@comcast.net
 Attn: Joseph Gaffney
 [] Phone (707) 823-2151 [] , **PERMITTEE**

In compliance with your request of February 1, 2021, we are hereby amending the above-numbered encroachment permit as follows:


Date of completion extended to: February 28, 2022

Reference your permit:

Encroach within State’s right-of-way to install (4) Parklets to accommodate outdoor dining in areas for General Public Use to offset negative effects on local business from Covid-19 safety and shelter orders. First Parklet will be on the west side of Highway 116 and N. Main Street just south of Mckinley Street, the second Parklet will be on the same block, west side at the southern, other end just north of Bodega Ave, The third Parklet will be on Highway 116 and S. Main Street on the west side, north of Burnett Street, and the fourth Parklet will be at the northwest corner of the intersection of Highway 116 and Petaluma Ave and Highway 12 and Sebastopol Ave, on State Highways 04-SON-116, Post Mile 26.733, in the City of Sebastopol.

A minimum of 7 days prior to the start of work under this encroachment permit, notice must be given to State Representative Augusto Lumba, 1009 Clegg Court, Suite D, Petaluma, CA 94954, at augusto.lumba@dot.ca.gov or (510) 579-2632, weekdays between 7:00 a.m. and 3:30 p.m., excluding holidays.

Except as amended, all other terms and provisions of the original permit shall remain in effect.

RF cc: State Rep: augusto.lumba@dot.ca.gov Maint: will.hauke@dot.ca.gov DTM: seyed.nedjad@dot.ca.gov TMC: D4TMC/D04/Caltrans/CAGov File: 04-20-N-MC-2398	APPROVED: DINA EI-TAWANSY, Acting District Director BY:  CHRIS MASTER, Senior Permit Engineer
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ADA Notice

For individuals with sensory disabilities, this document is available in alternate formats. For information call (916) 654-6410 or TDD (916) 654-3880 or write Records and Forms Management, 1120 N Street, MS-89, Sacramento, CA 95814.

D4 Encroachment Permit Work Scheduling Request Form

Submit your request to schedule traffic control weekly, 7 days in advance, using this form. Submit your request to State Representative (Inspector) listed on page 1 or 2 of your permit. If your inspector is not available, contact Permit Duty Station at (510) 286-4401. Check special provisions for authorized work hours. Any deviation from the permit must be in writing and requires additional review and approval.

INSTRUCTIONS AND ABBREVIATIONS: See the procedures on page 2 of this form.

- 1. Permit No.: _____ 2. Expiration Date: _____ 3. Request Date: _____
- 4. Caltrans Inspector: _____ 5. Requested Work Week: _____ to _____
- 6. Route: _____ 7. County: _____ 8. City or Township: _____
- 9. PostMiles: From: _____ To: _____ 10. Existing Lanes (in each Dir): Dir _____ Lns _____ / Dir _____ Lns _____
- 11. Describe Location (use landmark if necessary): From: _____ To: _____
- 12. Name of Conventional Highway or Surface St: _____
- 13. Fill in or 'x' if applicable (a through k): (a) Divided Hwy or Undivided Hwy (b) Full-Closure 1 dir or both dir
 (c) One-Way Traffic Control: Only on "Undivided" Hwy (Alternate use of same lane for both directions--hold traffic 5-10 min w/flaggers)
 (d) Connector Ramp: (State Highway #) _____ to (State Highway #) _____ Closed or Lane # _____
 (e) Off-ramp: (Freeway to City St.) Ramp Name: _____ Off-ramp Closed or Lane#: _____
 (f) On-ramp: (City St. to Freeway) Ramp Name: _____ On-ramp Closed or Lane#: _____
 (g) Divert Traffic or Contra Flow: Reconfigure Lns/divert traffic to Lane# ___ in the ___ Direction; ___ Lns open each dir
 (h) Intermittent Traffic Control (i) Various Locations (j) Long-Term (24+ hours continuous) ETO

(k) Year:		Time		Dir		* * * * * Restricted Lanes * * * * *														Brks		Closure ID#				
From DATE	To DATE	DAY(S) SU-M-T-W-TH-F-SA	24-HR CLOCK		NB	SB	Full Closure See Detour	SHLDR		1	2	3	4	5	6	V	Aux or Coll	CD or Med	TURN PCKT(S)		Park Strip	5 to 15 min	Rolling	Caltrans will complete & return		
			Start (10-97)	Finish (10-98)				EB	WB										L	R					L	R

14. Description of work/comments: _____

15. Detour (Required for full closure): _____

16. Contingency Plan: _____

17. On-site during work (circle if applicable): CHP / PD / Other: _____

18. Name:	Permittee or Permittees Representative/Contractor:	
	Address including zip code:	
On-site Personnel Contact Name of person in responsible charge & phone number(s).	Name:	
	Email:	
	Office:	FAX:
	Cell:	Emergency phone number 24/7:

19. **"REAL-TIME" STATUS INSTRUCTIONS – PLEASE MAKE YOUR FIELD PERSONNEL AWARE & RESPONSIBLE!**
 Permittee must **STATUS** lane closures **DAILY** via Caltrans District 4's 24-Hour Communication Center at (510) 286-6359. Status using Closure ID Number when work begins, to **1097** (1st cone down), and again to **1098** (last cone picked up); OR, **1022** to cancel. Any **delay** in picking up your closure must be reported immediately.

D4 Encroachment Permit Work Scheduling Procedures

1. **INSTRUCTIONS:** Fill in blanks or check appropriate boxes. Attach maps or diagrams, if available. Enter **beginning day** through **ending day of work week** (M-T-W-TH-F-SA-SU). **Date: Month/Day**—Enter month (01-12) and day (01-31) of requested week. **Start & Finish Time:** Use 24-hour clock format. **Read** the Permit Special Provisions for **hours & days** allowed. Separate lane closure #'s are required for each direction and facility. Use separate line for each. **Lanes** are numbered in direction of travel from left to right, excluding turn pockets; left being #1 or "fast lane." Check boxes under **Restricted Lanes** to indicate lanes or parts of highway to be closed. **VL** may be checked with note in Comments Section stating number of lanes to remain open at all times.
2. **ABBREVIATIONS:** **Aux**=auxiliary, **CD**=Center Divide; **Coll**=Collector; **Contra Flow**=Close 1 direction of traffic and divert to lane(s) in opposite direction or a turn lane. **DAY(S)**=(M-T-W-TH-F-SA-SU); **Dir**=Direction (**NB**=Northbound, **SB**=Southbound, **WB**=Westbound, **EB**=Eastbound); **ETO**=Emergency Traffic Operations; **F/L**=fog line; **Hwy**=Highway; **Lns**=Lanes; **L**=Left; **Med**=Median; **Park Strip**=Parking area parallel to lane; **PCKT**=Pocket; **Rolling**=traffic breaks for closure such as sweeping; **R**=Right; **SHLDR**=Shoulder; **VL**=Various Lanes
3. Requests for scheduling must be submitted on this form to the Inspector listed on page 1 or 2 of your permit. If your inspector is not available, contact Permit Duty Station at (510) 286-4401.
4. All permitted work (**with or without traffic control**) is subject to advance scheduling on this form, seven (7) days in advance of the work week requested. Submittals and approvals must continue on a weekly basis.
5. If work begins weekly on Sunday, the work week must be Sunday through Saturday. If work week begins on Monday, the work week must be Monday through Sunday.
6. Incomplete, illegible, or inaccurate requests may be returned for correction. Assistance for completing the request may be obtained from the designated State Representative.
7. Every attempt will be made to return timely requests with closure ID or work authorization numbers, to the Permittee by close of business on Thursday, prior to the scheduled work week. When deemed necessary to ensure public convenience, Caltrans may deny and/or reschedule the request.
8. All requests must include a contingency plan for restoring public traffic (i.e. reopening of a closed lane, ramp and/or shoulder) in the event of (1) CHP or the local authority requires opening due to an unforeseeable incident in the nearby vicinity, or (2) permitted experiences an equipment breakdown, shortage of or lack of production materials or any other failure which would otherwise delay restoring public convenience within the time limits specified in the permit. The contingency plan must include availability of any proposed standby equipment and stockpiled materials that can be utilized for the immediate opening of closures when ordered by the State representative. Acceptance of the contingency plan by the Engineer must not relieve the Contractor from the requirement of opening the restricted travel way to accommodate public traffic as specified in the lane closure hour's section of the permit provisions.
9. Caltrans will review and process the request by entering all information into the Statewide Lane Closure System (LCS). This process generates a work authorization number*. This number will be entered on the request form and returned to Permittee as approval to proceed AND will be used to "**Real-Time Status**" on a daily basis. Permittee must communicate with Caltrans 24-hour District 4 Communication Center (DCC) via telephone at **(510) 286-6359** twice daily when working, or once daily if cancelled.
 - a. When work begins (1st cone down), Permittee must contact Caltrans DCC and relay: "**(Closure ID #*) is 1097.**"
 - b. When work ends (last cone picked up), Permittee must contact Caltrans DCC and relay: "**(Closure ID #*) is 1098.**"
 - c. If the work is cancelled on any scheduled day, Permittee must contact Caltrans DCC and relay; "**(Closure ID #*) is 1022.**" A "10-22" (cancellation) can be phoned any time before the scheduled "10-97" time, but no later than 1 hour prior to scheduled "10-98" time.
 - d. During the work, any unexpected occurrences including delayed openings, accidents, etc., must be communicated to Caltrans DCC immediately.

Avoid possible miscommunication when calling status. Use the **PHONETIC ALPHABET** to state your Closure ID:
A=Adam, **B**=Boy, **C**=Charles, **D**=David, **E**=Edward, **F**=Frank, **G**=George, **H**=Henry, **I**=Ida, **J**=John, **K**=King,
L=Lincoln, **M**=Mary, **N**=Nora, **O**=Ocean, **P**=Paul, **Q**=Queen, **R**=Robert, **S**=Sam, **T**=Tom, **U**=Union, **V**=Victor, **W**=William,
X=X-ray, **Y**=Yellow, **Z**=Zebra. *Example: P82CA="Paul 82 Charles Adam"*
10. The intent of these procedures is to help ensure public convenience by identifying planned closures on the State Highway system, resolving potential conflicts, and disseminating all available "**REAL-TIME**" information via the traffic media to all motorists, including but not limited to the public, CHP, local police and sheriffs' office, and emergency fire and rescue personnel.

* "closure ID number" is the same as "work authorization number"

PERMIT NO.

Dear Sir or Madam:

*All work authorized by the above-numbered permit was
completed on* _____
DATE

SIGNATURE OF PERMITTEE

FM 92 1546 M

PERMIT NO.

Dear Sir or Madam:

*All work authorized by the above-numbered permit was
completed on* _____
DATE

SIGNATURE OF PERMITTEE

FM 92 1546 M

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DATE

SIGNATURE OF PERMITTEE

FM 92 1546 M

STATE OF CALIFORNIA • DEPARTMENT OF TRANSPORTATION
ENCROACHMENT PERMIT

TR-0120 (REV 6/2012)

Permit No. **Agenda Item Number 12**
04-20-N-MC-2398

In compliance with (Check one):

- Your application of October 16, 2020
- Utility Notice No. _____ of _____
- Agreement No. _____ of _____
- R/W Contract No. _____ of _____

TO: City of Sebastopol
 7120 Bodega Avenue
 Sebastopol, CA 95472
 Email: Hmikus@cityofsebastopol.org
 Attn: Henry Mikus
 Phone (707) 328-8105 , **PERMITTEE**

Dist/Co/Rte/PM 04/SON/116/26.733	
DATE October 19, 2020	
Fee Paid \$Exempt	Deposit \$
Performance Bond Amount \$	Payment Bond Amount \$
Bond Company	
Bond Number (1)	Bond Number (2)

and subject to the following, **PERMISSION IS HEREBY GRANTED** to:
 Encroach within State's right-of-way to install (4) Parklets to accommodate outdoor dining in areas for General Public Use to offset negative effects on local business from Covid-19 safety and shelter orders. First Parklet will be on the west side of Highway 116 and N. Main Street just south of Mckinley Street, the second Parklet will be on the same block, west side at the southern, other end just north of Bodega Ave, The third Parklet will be on Highway 116 and S. Main Street on the west side, north of Burnett Street, and the fourth Parklet will be at the northwest corner of the intersection of Highway 116 and Petaluma Ave and Highway 12 and Sebastopol Ave, on State Highways 04-SON-116, Post Mile 26.733, in the City of Sebastopol.

A minimum of 7 days prior to the start of work under this encroachment permit, notice must be given to State Representative Augusto Lumba, 1009 Clegg Court, Suite D, Petaluma, CA 94954, at augusto.lumba@dot.ca.gov or (510) 579-2632, weekdays between 7:00 a.m. and 3:30 p.m., excluding holidays.

THIS PERMIT IS NOT A PROPERTY RIGHT AND DOES NOT TRANSFER WITH THE PROPERTY TO A NEW OWNER.


The following attachments are also included as part of this permit (<i>Check applicable</i>):		In addition to fee, the Permittee will be billed actual costs for:	
<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> No	General Provisions (TR-0045) _____	
<input type="checkbox"/> Yes	<input checked="" type="checkbox"/> No	Utility Maintenance Provisions () _____	
<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> No	Special Provisions (TR-0400) _____	
<input type="checkbox"/> Yes	<input type="checkbox"/> No	A Cal-OSHA permit, if required: Permit No. _____	
<input type="checkbox"/> Yes	<input checked="" type="checkbox"/> No	As-Built Plans Submittal Route Slip for Locally Advertised Projects	
<input type="checkbox"/> Yes	<input checked="" type="checkbox"/> No	Water Pollution Control Documents (SWPPP/WPCP)	
<input type="checkbox"/> Yes	<input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes	<input checked="" type="checkbox"/> No
		<input type="checkbox"/> Yes	<input checked="" type="checkbox"/> No
		Review	
		Inspection	
		Field Work	
<i>(If any Caltrans effort expended)</i>			

Yes No The information in the environmental documentation has been reviewed and considered prior to approval of this permit.

This permit is void unless the work is completed before February 28, 2021

This permit is to be strictly construed and no other work other than specifically mentioned is hereby authorized.
 No project work shall be commenced until all other necessary permits and environmental clearances have been obtained.

Permit Writer: khon.tram@dot.ca.gov
 cc: State Rep.: augusto.lumba@dot.ca.gov
 Maint.: will.hauke@dot.ca.gov
 DTM: thet.oo@dot.ca.gov
 TMC: D4TMC/D04/Caltrans/CAGov
 File:

APPROVED:
DAVID SALLADAY, District Permit Engineer
 BY:

CHRIS MASTER, Senior Permit Engineer

ADA Notice

For individuals with sensory disabilities, this document is available in alternate formats. For information call (916) 654-6410 or TDD (916) 654-3880 or write Records and Forms Management, 1120 N Street, MS-89, Sacramento, CA 95814.

ADDITIONAL PERMIT PROVISIONS:

In addition to the attached "Encroachment Permit General Provisions" (TR-0045) and "Storm Water Special Provisions for Minimal or No Impact" (TR-0400) (available at <http://dot.ca.gov/programs/traffic-operations/ep/ep-manual>), all work permitted herein must comply with the following provisions:

The permittee shall comply with the code of safe practices (available at <https://dot.ca.gov/-/media/dot-media/programs/construction/documents/construction-standards/code-of-safe-practices-sept-2017.pdf>), when working within the State right-of-way.

2018 Caltrans Standard Specifications and Standard Plans (available at <https://dot.ca.gov/programs/design/ccs-standard-plans-and-standard-specifications>)

Notwithstanding General Provision 35, lane closures and other activities that may cause a traffic impact requires the permittee to apply for and obtain a closure ID prior to the start of work. Requests must be submitted using the attached "Encroachment Permit Work Scheduling Request Form."

Shoulder may be closed and parking restricted.

Time extension requests must be made a minimum of 2 weeks prior to permit expiration.

The Permittee by accepting this permit accepts full and sole responsibility for all installations and, associated claims and damages. The permittee is solely responsible for all activities/actions by its agents/contractors associated with any authorized activity on the State Highway System.

This temporary permit is to address the unique circumstances created due the March 13, 2020, COVID-19 State of Emergency declaration by the United States federal government and the California Governor on March 4, 2020, proclamation of a State of Emergency regarding the threat of COVID-19.

This permit is non-transferable. Only the Permittee to whom this permit was issued is to perform the services authorized herein and shall always have this permit in their possession when performing said services. This permit must be presented for inspection upon demand by any State Representative, or Law Enforcement Official.

To help serve the public interest, Caltrans is only authorizing installations to facilitate physical distancing and other safety measures to prevent the spread of COVID-19 during the current State of Emergency.

Use of State Highway System right-of-way for private use/gain is prohibited by Streets and Highways Code 731 and California Vehicle Code 22520.5. By accepting this permit, the permittee is accepting the responsibility to ensure no such activities occur on State Highway System right-of-way, including but not limited to vending, business services, purchases, sales transactions, seating customers in the public seating area, or other uses of the State Highway System for private use/gain.

Advertising in any form is prohibited on State Highway System right-of-way by Code of Federal Regulations Title 23, Part 750 and California Business & Professions Code 5403. This includes sandwich boards, posters or any other form of advertising.

This permit will expire on the date listed on the first page of this permit or at the end of the declared State of Emergency regarding COVID-19, whichever occurs first.

This permit authorizes:

- a. Label 6 feet spacing on the sidewalk to enable physical distancing while entering restaurants or other business along State Highway:
 - i. Route XXXX between Street AAA (PM ____) and Street BBBB (PM ____)
 - ii. Route XXXX between Street AAA (PM ____) and Street BBBB (PM ____)
 - iii. Route XXXX between Street AAA (PM ____) and Street BBBB (PM ____)
- b. Post "No Parking" signs prohibiting curb side parking in existing curb side parking spaces. Necessary signs or other temporary traffic control devices in accordance with CA-MUTCD can be placed to enable curbside pickup for business along the highways and within limits listed above. The permittee is responsible for ensuring the curbside pickup activities do not impact the safety or operations on the State Highway System. No temporary

or fixed objects (Tables etc.) can be placed on sidewalk along parking spaces designated to assist or serve curbside pickup activities.

Temporary canopies can be installed to provide shade over sidewalk. They must comply with the requirements established for “Arcades” established in Section 501.3b of Encroachment Permit Manual and all of these conditions:

- a. Conforms to local building code.
- b. Structurally adequate.
- c. Overhang may not extend closer than 24 inches horizontally from the curb face. Exceptions are in historical districts where overhangs are permissible to the curb face.
- d. Preferred minimum vertical clearance from the sidewalk is 12 feet. A minimum 8 feet clearance is acceptable when local codes are satisfied.
- e. Must not interfere with or hide any traffic control devices (traffic signals, traffic signs, etc.).

Public seating area must be open for use by the general public and support the needs of local communities. Public seating area must include signage designating the area for use by the general public and not reserved for any patrons of the adjacent businesses or specific business.

- a. Vending, sales, transactions, table service, and similar activities in the public seating area are strictly prohibited. No portion of a transaction may take place within the State Highway System right-of-way. For example, the public may purchase takeout/ “to-go” food at a business and use the public seating area to consume the food, but the business may not conduct the sale of the food or accept payment for the food while in the public seating area.
- b. Public seating area may include temporary benches, tables, chairs or other seating, bike racks, and planting areas, but must not include any fixed object. Temporary tables and seating must be distinct from those of nearby businesses and must not include the same or similar color scheme, marking, logo, symbol, or any other identifier of any particular business.
- c. All installations must be heavy enough, or other measures be implemented, to ensure they do not move onto the travel lanes, highway users, or business properties, or obstruct the minimum required accessible path of travel on the sidewalk.
- d. Public seating area must be installed to conform with the Americans with Disabilities Act Accessibility Guidelines (available at <https://www.access-board.gov/guidelines-and-standards/buildings-and-sites/about-the-ada-standards/ada-standards>).
- e. Public seating area may include a sign identifying the local public entity permittee. All forms of advertising are prohibited in the public seating area and in the curbside pickup area.
- f. The permittee must ensure that no fee is being charged for the use of the public seating area or the curbside pickup area.
- g. Public seating area must be properly maintained and kept free of trash or nuisance.
- h. Following sign, large and legible enough to read easily by users, must be posted at the public seating areas: “Consumption of Alcohol is Prohibited”.

Markings on sidewalk (s) for physical distancing:

- a. Only non-permanent removable tape may be used to mark the 6-foot social distancing spacing on the sidewalk.
- b. Size of the markings must be no larger than 12 inches by 12 inches.
- c. Permanent markings using paint or other means are prohibited.
- d. Markings must not be a tripping or slipping hazard.
- e. When the markings are no longer required or when the State of Emergency ends, the markings must be removed.

- f. Markings must not interfere with traffic control devices. Markings must not resemble any traffic control devices.
- g. Markings must be of plain solid color and must not include any messages, advertising, symbols etc.

Closure of travel lanes or full sidewalk is prohibited.

The permittee is solely responsible for compliance with the Americans with Disabilities Act (ADA). Adequate clearance in accordance with Caltrans Design Information Bulletin 82 (DIB 82) must be maintained through the sidewalk to serve pedestrians. DIB 82 can be accessed at <https://dot.ca.gov/-/media/dot-media/programs/design/documents/dib82-06-11y.pdf>.

The encroachments on State Highway System right-of-way shall not negatively impact the safety of highway users, including but not limited to motorists, bicyclists, and pedestrians, and any person legally using the State Highway System right-of-way. The encroachments shall not negatively impact highway operations or maintenance of State Highway System right-of-way.

The permittee must ensure all objects placed in connection with the authorized activities do not create a tripping hazard or other hazard to any person or property.

The permittee shall not construct nor place on the State Highway System right-of-way any improvement which would impair Caltrans' ability to maintain, operate, use, repair, or improve any part of the transportation facility in State Highway System right-of-way. The permittee is responsible for promptly removing, at its sole cost and expense, any improvement which Caltrans identifies as a hazard to or impairment of a transportation facility.

The permittee shall treat all businesses equally and any measures implemented by or on behalf of the permittee on State Highway System must be fair and serve all businesses equally. The permittee must handle and resolve any and all complaints. Permittee may not attach anything to the travel way or highway facilities/structures (signs, sidewalk etc.) by bolt, screw or any other invasive connection type.

No signs are allowed to be posted on any State-owned warning guide or regulatory sign posts. Permittee is responsible for removing all location signs posted within the State right of way.

Caltrans may revoke the permit at any time, for any or no reason, Common reason for revocation would be violation of the terms of this permit, operational needs, or safety issues.

The permittee must not hook-up or connect into any Caltrans owned utilities. The permittee is expected to independently procure needed utility services and infrastructure like necessary temporary lighting fixtures, etc.

Permitted lighting fixtures, mirrors, reflectors, and supports must not present a glare or other safety hazard.

The permittee shall assign a liaison to coordinate and oversee the authorized activities under this permit. The liaison shall always be available to address any and all issues/requests associated with the permit.

The permittee is solely responsible for the objects (i.e., canopies, tables, chairs, benches etc.) and securing all object installations.

The permittee and its agents, employees, representatives, and contractors shall be solely responsible for installing, operating, and maintaining the temporary installations authorized by the permit. The permittee and its agents, employees, representatives, and contractors shall ensure that the objects placed within State Highway System right-of-way are properly serviced, maintained, and repaired.

Any traffic control required shall receive prior approval from the Caltrans representative. If Caltrans standard traffic control plans are not adaptable for the required traffic control needs, site-specific traffic control plans stamped and signed by a California Licensed Engineer shall be submitted along with a rider permit request, for review and approval prior to using such site-specific traffic control plans. Standard plans may be referenced and are available at <https://dot.ca.gov/programs/design/ccs-standard-plans-and-standard-specifications>.

All traffic control devices (signs, flagging, flags, PCMSs and other devices) shall conform to the requirements set forth in the latest California Manual on Uniform Traffic Control Devices (CA-MUTCD) and Caltrans Standard Plans and Specifications.

The permittee shall monitor and maintain all traffic control devices for the entire duration of this permit. At least one person shall be assigned by the permittee to provide full-time maintenance of traffic control devices.

No excavation or ground disturbance is allowed. All installations allowed must be above ground installations only.

The permittee shall not attach nor temporarily affix any item on Caltrans facilities or infrastructure, unless otherwise expressly allowed in this permit.

The permittee is responsible for ensuring the sidewalk and curbside pickup areas on State Highway System is free of trash, within the limits and locations authorized under this permit.

The permittee shall restore the State Highway System right-of-way to pre-existing condition, which includes the pickup, removal, and disposal of all litter, materials, and other items related to the permittee's activities to Caltrans' satisfaction.

Upon completion of work authorized by this encroachment permit, the permittee must provide the State Representative with "Notice of Completion" (TR-0128) (available at <https://dot.ca.gov/-/media/dot-media/programs/traffic-operations/documents/encroachment-permits/tr0128.pdf>.)

The State Representative and the CHP reserve the right to require the reopening of the highway at any time as necessary. All cost must be borne by the permittee.

Changes to the provisions herein require an Encroachment Permit Rider, except for minor changes authorized by the State Representative.

The State Representative or CHP may stop work that is not being performed in strict compliance with the conditions of this permit.

Neither materials nor waste must be stockpiled within the State highway right-of-way.

Permittee must strictly comply with conditions of this permit. Minor variations require written State Representative approval; changes require a Caltrans Permit Rider.

Before any State highway closure is to begin which will interrupt the normal flow of public traffic, an approval shall be obtained from State Representative.

A pre-job meeting with the State Representative is required at least 7 days prior to the start of any work under this permit. Failure to do so may result in permit revocation with no prejudice.

The permittee must provide the stage construction plans, traffic handling plans, work schedule, and a list of all sub-contractors to the State Representative at the time of the pre-job meeting.

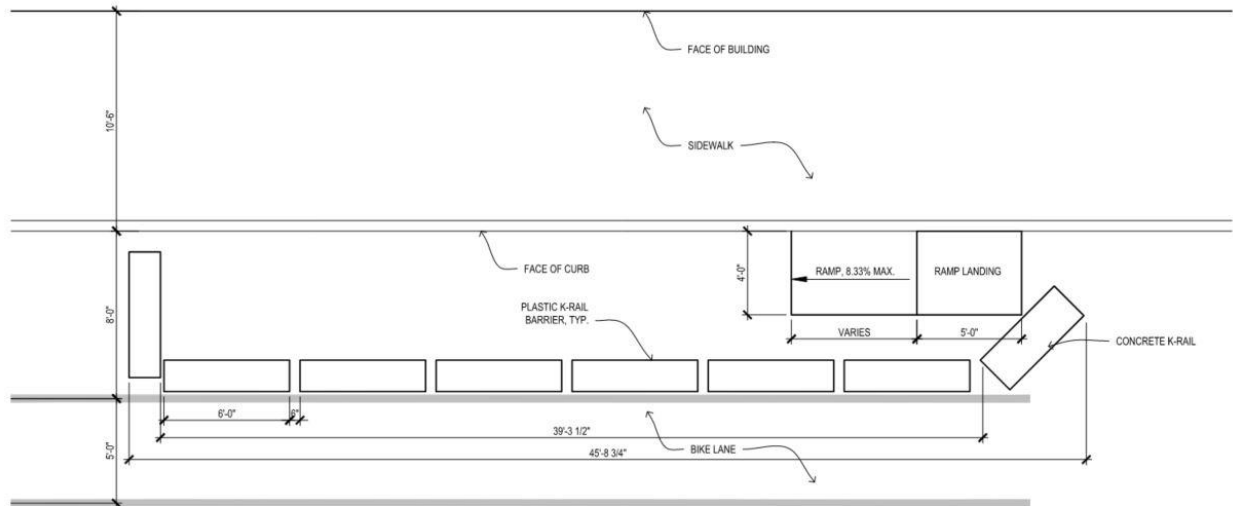
No lane may be closed or obstructed at any time unless specifically allowed per the encroachment permit, shown in approved traffic control plans, and/or as directed by the State Representative.

Sebastopol Temporary Parklet Application

The City of Sebastopol would like to create several parklets in the downtown area to help provide additional space for people to be while there are limits on the number of people allowed inside downtown restaurants and stores. Each parklet will be furnished with tables, chairs, umbrellas and other shade structures. These furniture elements would be provided by downtown businesses. The businesses will also be responsible for making sure the parklets are maintained. It is understood that the parklet furnishing must be distinct from that of nearby businesses and that parklets are for general public use. The map below shows the proposed locations.



The parklets would be temporary and would use a combination of plastic and concrete k-rails to delineate the parklet space. Each location is described below. Address locations are approximate. Parking spaces are not marked on Main Street. A 'typical' site plan is included for reference. Locations 1-3 would use this layout.



Location 1 – 124-130 South Main Street

This location would include 7 plastic k-rails and one concrete k-rail at the leading edge. A wood ramp will be provided from the curb to the street surface for accessibility. The proposed ramp would comply with Caltrans standard T34, Option C.

Location 2 - 112-122 N. Main Street

This location would include 7 plastic k-rails and one concrete k-rail at the leading edge. A wood ramp will be provided from the curb to the street surface for accessibility. The proposed ramp would comply with Caltrans standard T34, Option C.

Location 3 - 156-162 N. Main Street

This location would include 7 plastic k-rails and one concrete k-rail at the leading edge. A wood ramp will be provided from the curb to the street surface for accessibility. The proposed ramp would comply with Caltrans standard T34, Option C.

Location 4 - 6902-6914 Sebastopol Ave.

This location would include blocking both ends of a short segment of Sebastopol Ave. between a landscape island and the sidewalk. K-rails would block both ends of the section. The k-rails at the east side would be behind the crosswalk. Accessible access would be provided via the crosswalk and curb ramps located at each end.

Existing conditions photos of each location are included on the following pages.

Sebastopol Temporary Parklet Application – Existing Condition Photos

Location 1 – 124-130 S. Main Street

(1) Sidewalk Left



(1) Street Left



(1) Street Front



(1) Street Right



(1) Sidewalk Right



Sebastopol Temporary Parklet Application
Location 2 – 112-122 N. Main Street

(2) Sidewalk Left



(2) Street Left



(2) Street Front



(2) Street Right



(2) Sidewalk Right



Sebastopol Temporary Parklet Application
Location 3 – 156-162 N. Main Street

(3) Sidewalk Left



(3) Street Left



(3) Street Front



(3) Street Right



(3) Sidewalk Right



Sebastopol Temporary Parklet Application
Location 4 – 6902-6914 Sebastopol Ave.

(4) Sidewalk Left



(4) Street Left-1



(4) Street Left-2



(4) Street Front



(4) Street Right-1



(4) Street Right-2



(4) Street Right-3



(4) Sidewalk Right



NOTES:

1. Only signs related to pedestrians are shown. For all other signs see appropriate T-sheets.
2. Barricades closing sidewalk shall cover the full width of the sidewalk. Use R9-11 sign when there are destination points between the detour and the work area. Locate the R9-11 sign to allow pedestrian access.
3. Advance warning sign is not required if the work area is within the limits of a larger work zone. Sign shall be equipped with at least two flags for daytime closure. Each flag shall be orange or fluorescent red-orange in color.

NOTES:

See Standard Plan T9 for tables.
 Use cone spacing X for taper segment, Y for tangent segment or Z for conflict situations, as appropriate, per Table 1 unless X,Y, or Z cone spacing is shown on this sheet.

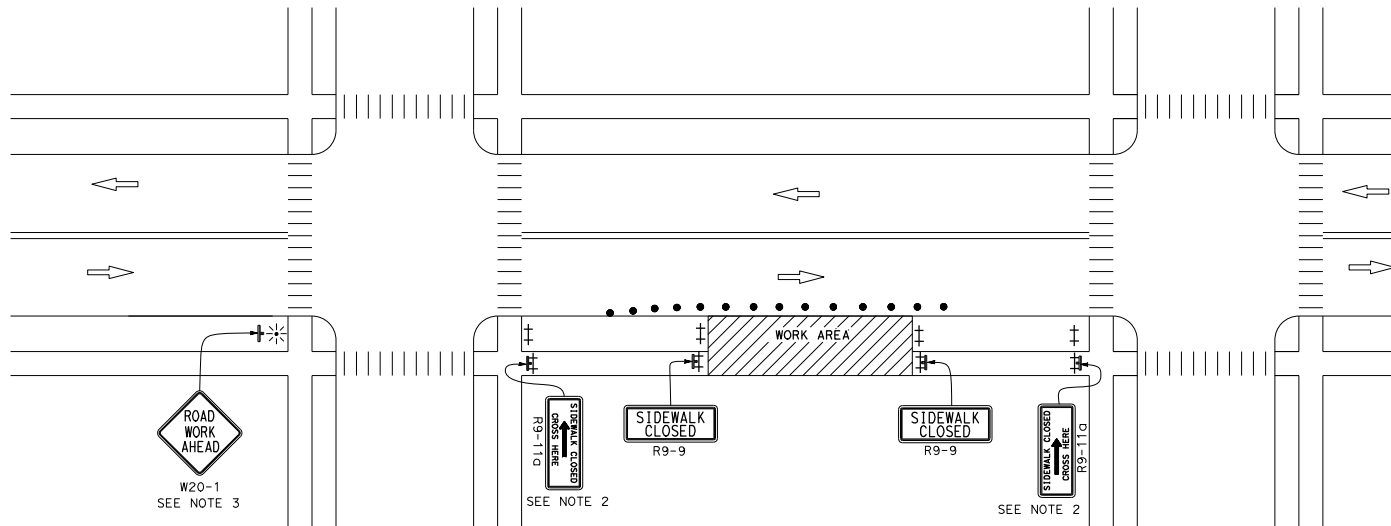
DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET NO.	TOTAL SHEETS

Atifa Ferouz
 REGISTERED CIVIL ENGINEER

May 31, 2018
 PLANS APPROVAL DATE

Atifa Ferouz
 No. C80402
 Exp. 3-31-19
 CIVIL
 STATE OF CALIFORNIA

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LEGEND:

- † BARRICADE
- TRAFFIC CONE
- ✱ PORTABLE FLASHING BEACON
- † SIGN
- † TEMPORARY TRAFFIC CONTROL SIGN ON BARRICADE

SIGN PANEL SIZE (Min)

SIGN DESIGNATION	SIGN OR PLAQUE	SIGN SIZE
R9-9	SIDEWALK CLOSED	24" x 12"
R9-11	SIDEWALK CLOSED AHEAD CROSS HERE	24" x 18"
R9-11a	SIDEWALK CLOSED CROSS HERE	24" x 12"
W20-1	ROAD WORK AHEAD	36" x 36"

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2018 STANDARD PLAN T30

STATE OF CALIFORNIA
 DEPARTMENT OF TRANSPORTATION
**TEMPORARY PEDESTRIAN ACCESS ROUTES
 TYPICAL SIDEWALK CLOSURE
 AND PEDESTRIAN DETOUR**

NO SCALE

T30

1-22-18

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NOTES:

1. Only signs related to pedestrians are shown. For all other signs see appropriate T-sheets.
2. Separate pedestrian walkway from traffic and work zone activities, when temporary walkway is adjacent to traffic.
3. The temporary pedestrian access route must not lead into conflict with vehicles or work.
4. Advance warning sign is not required if the work area is within the limits of a larger work zone. Sign shall be equipped with at least two flags for daytime closure. Each flag shall be orange or fluorescent red-orange in color.
5. All devices used to channelize pedestrian flow must connect such that gaps do not allow pedestrians to stray from the channelized path.
6. Barricades closing sidewalk shall cover the full width of the sidewalk.
7. Separate the temporary pedestrian access route from traffic using a temporary barrier and a crash cushion if necessary.
8. When it is not possible to maintain a minimum of 60 inches throughout the length of the pedestrian route, maintain a minimum width of 48 inches and provide a 60 X 60-inch passing space at least every 200 feet.
9. See Standard Plan A88A for detectable warning surface for curb ramps to apply to temporary curb ramps.
10. See Standard Plan T34 for temporary curb ramp options.

NOTES:

See Standard Plan T9 for tables.
Use cone spacing X for taper segment, Y for tangent segment or Z for conflict situations, as appropriate, per Table 1 unless X, Y, or Z cone spacing is shown on this sheet.

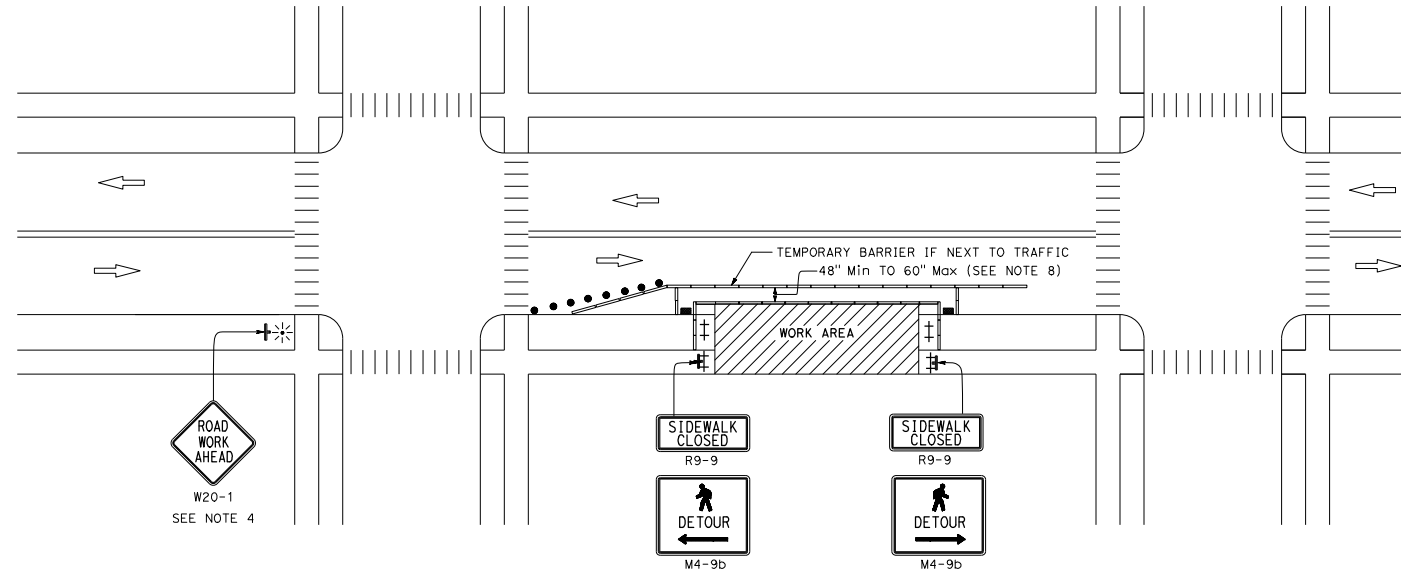
Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET NO.	TOTAL SHEETS

Atifa Ferouz
REGISTERED CIVIL ENGINEER

May 31, 2018
PLANS APPROVAL DATE

Atifa Ferouz
No. C80402
Exp. 3-31-19
CIVIL
STATE OF CALIFORNIA

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LEGEND:

- ⊥ BARRICADE
- TEMPORARY CURB RAMP
- ▭ CHANNELIZING DEVICE
- TRAFFIC CONE
- ⊛ PORTABLE FLASHING BEACON
- ⊥ TEMPORARY TRAFFIC CONTROL SIGN
- ⊥ TEMPORARY TRAFFIC CONTROL SIGN ON BARRICADE

SIGN PANEL SIZE (Min)

SIGN DESIGNATION	SIGN OR PLAQUE	SIGN SIZE
M4-9b	PEDESTRIAN DETOUR	30" x 24"
R9-9	SIDEWALK CLOSED	24" x 12"
W20-1	ROAD WORK AHEAD	36" x 36"

STATE OF CALIFORNIA
DEPARTMENT OF TRANSPORTATION

**TEMPORARY PEDESTRIAN ACCESS ROUTES
TYPICAL SIDEWALK DIVERSION WITHIN ROADBED**

NO SCALE

T31

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2018 STANDARD PLAN T31

1-22-18

[Return to Table of Contents](#)

NOTES:

1. Only signs related to pedestrians are shown. For all other signs see appropriate T-sheets.
2. Barricades closing sidewalk shall cover the full width of the sidewalk. Use R9-11 sign when there are destination points between the detour and the work area. Locate the R9-11 sign to allow pedestrian access.
3. Advance warning sign is not required if the work area is within the limits of a larger work zone. Sign shall be equipped with at least two flags for daytime closure. Each flag shall be orange or fluorescent red-orange in color.

NOTES:

See Standard Plan T9 for tables.
Use cone spacing X for taper segment, Y for tangent segment or Z for conflict situations, as appropriate, per Table 1 unless X,Y, or Z cone spacing is shown on this sheet.

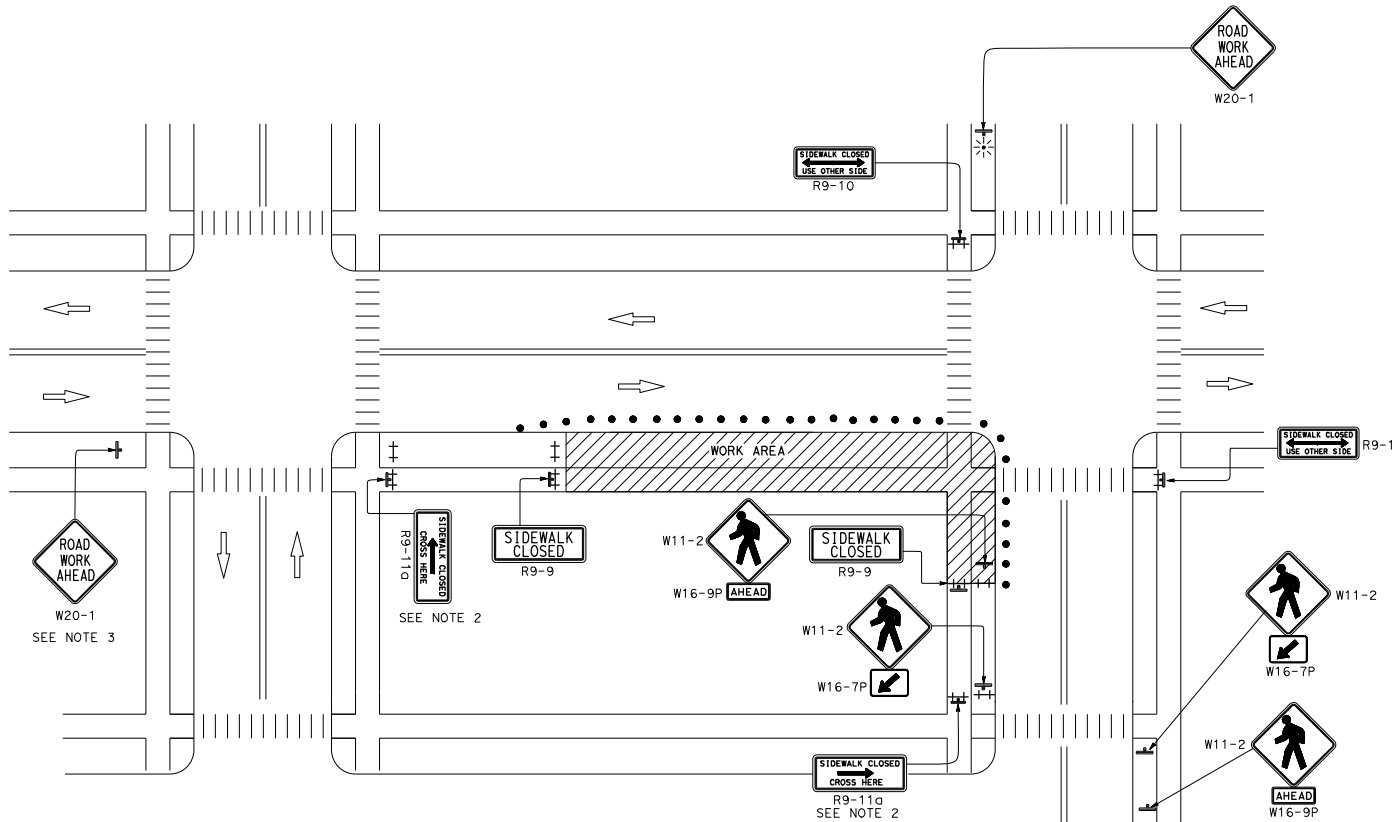
Dist	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET TOTAL SHEETS

Atifa Ferouz
REGISTERED CIVIL ENGINEER

May 31, 2018
PLANS APPROVAL DATE

Atifa Ferouz
No. C80402
Exp. 3-31-19
CIVIL

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LEGEND:

- † BARRICADE
- TRAFFIC CONE
- ⊛ PORTABLE FLASHING BEACON
- ⊥ TEMPORARY TRAFFIC CONTROL SIGN
- ⊥ TEMPORARY TRAFFIC CONTROL SIGN ON BARRICADE

SIGN PANEL SIZE (Min)

SIGN DESIGNATION	SIGN OR PLAQUE	SIGN SIZE
R9-9	SIDEWALK CLOSED	24" x 12"
R9-10	SIDEWALK CLOSED USE OTHER SIDE	24" x 12"
R9-11	SIDEWALK CLOSED AHEAD CROSS HERE	24" x 18"
R9-11a	SIDEWALK CLOSED CROSS HERE	24" x 12"
W11-2	PEDESTRIAN	36" x 36"
W16-7P	DIAGONAL DOWNWARD POINTING ARROW (PLAQUE)	24" x 12"
W16-9P	AHEAD (PLAQUE)	24" x 12"
W20-1	ROAD WORK AHEAD	36" x 36"

STATE OF CALIFORNIA
DEPARTMENT OF TRANSPORTATION

**TEMPORARY PEDESTRIAN ACCESS ROUTES
TYPICAL SIDEWALK/CROSSWALK CLOSURE
AND PEDESTRIAN DETOUR**

NO SCALE

T32

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2018 STANDARD PLAN T32

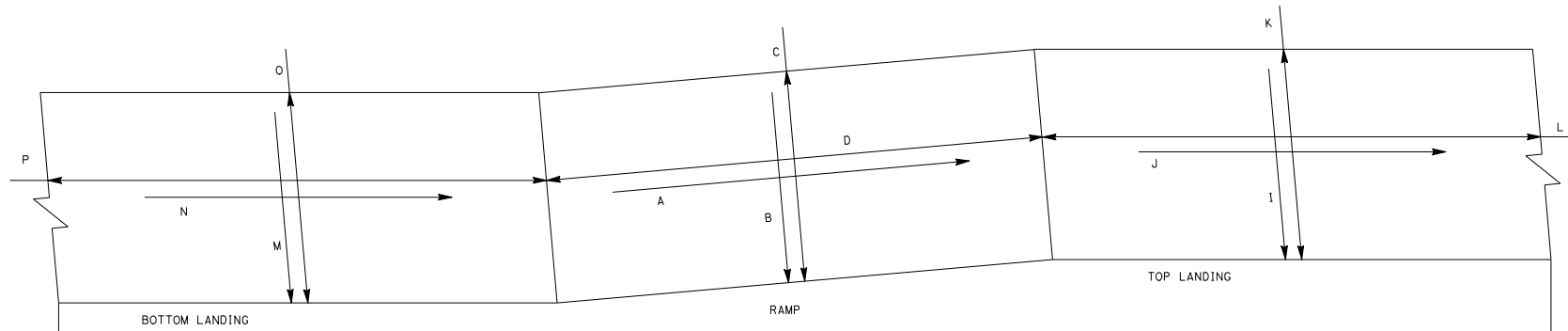
1-22-18

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RAMP				HAND RAIL		EDGE PROTECTION	
SLOPE	CROSS SLOPE	WIDTH	LENGTH	HEIGHT RIGHT SIDE	HEIGHT LEFT SIDE	RAIL RIGHT SIDE	RAIL LEFT SIDE
A	B	C	D	E	F	G	H
8.3% OR LESS	2.0% OR LESS	48 INCHES OR GREATER	30 FEET OR LESS	34 TO 38 INCHES	34 TO 38 INCHES	WITHIN 2 INCHES FROM GROUND	WITHIN 2 INCHES FROM GROUND
TOP LANDING				BOTTOM LANDING			
CROSS SLOPE	SLOPE	WIDTH	DEPTH	CROSS SLOPE	SLOPE	WIDTH	DEPTH
I	J	K	L	M	N	O	P
2.0% OR LESS	2.0% OR LESS	48 INCHES OR GREATER	60 INCHES OR GREATER	2.0% OR LESS	2.0% OR LESS	48 INCHES OR GREATER	60 INCHES OR GREATER

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET NO.	TOTAL SHEETS
<i>Atifa Ferouz</i> REGISTERED CIVIL ENGINEER No. C80402 PLANS APPROVAL DATE: May 31, 2018 EXP. 3-31-19 CIVIL STATE OF CALIFORNIA					
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STATE OF CALIFORNIA
 DEPARTMENT OF TRANSPORTATION
**TEMPORARY PEDESTRIAN
 ACCESS ROUTES**
RAMP
 NO SCALE

T33

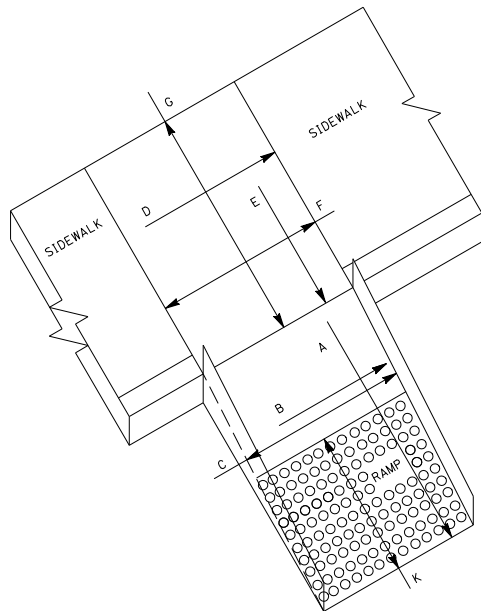
2018 STANDARD PLAN T33

1-22-18

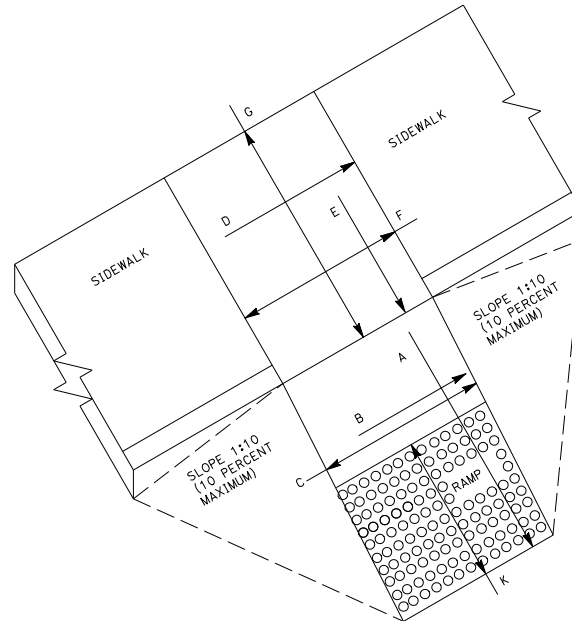
CURB RAMP			TOP LANDING				DETECTABLE WARNING SURFACE
SLOPE	CROSS SLOPE	WIDTH	CROSS SLOPE	SLOPE	WIDTH	DEPTH	DEPTH
A	B	C	D	E	F	G	K
8.3% OR LESS	2.0% OR LESS	48 INCHES OR GREATER	2.0% OR LESS	2.0% OR LESS	48 INCHES OR GREATER	60 INCHES OR GREATER	MINIMUM 36 INCHES

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET NO.	TOTAL SHEETS
Atifa Ferouz REGISTERED CIVIL ENGINEER No. C80402 Exp. 3-31-19 CIVIL STATE OF CALIFORNIA					
May 31, 2018 PLANS APPROVAL DATE					
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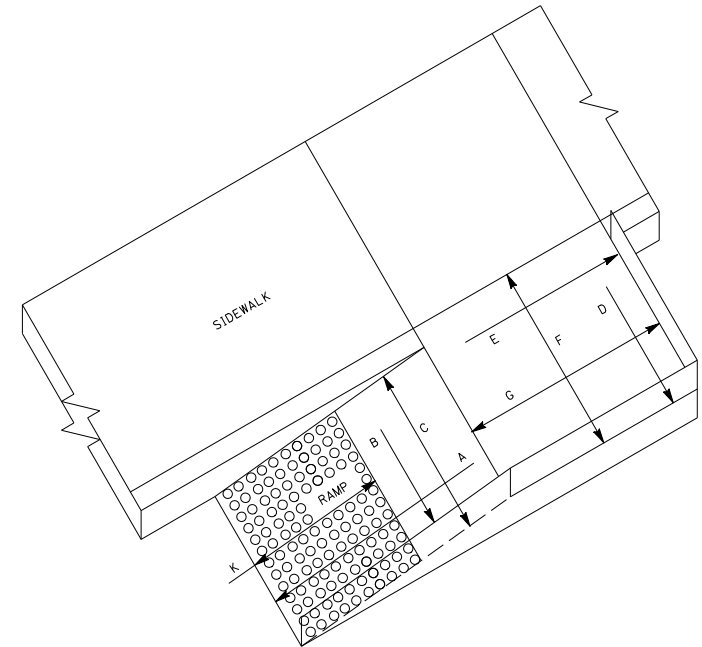
NOTES: If the above requirements cannot be met, on existing sites with space limitations, the following slopes are allowed:
 For a maximum rise of 6 inches a slope between 1:12 to 1:10 is allowed.
 For a maximum rise of 3 inches a slope between 1:10 to 1:8 is allowed.



OPTION A
SHOWN WITH SIDE EDGE



OPTION B
SHOWN WITH SIDE APRON



OPTION C
PARALLEL RAMP
SHOWN WITH SIDE EDGE

STATE OF CALIFORNIA
 DEPARTMENT OF TRANSPORTATION
**TEMPORARY PEDESTRIAN
 ACCESS ROUTES
 CURB RAMP OPTIONS**
 NO SCALE

T34

300

2018 STANDARD PLAN T34

1-22-18

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1. **AUTHORITY:** The California Department of Transportation ("Department") has authority to issue encroachment permits under Division 1, Chapter 3, Article 1, Sections 660 through 734 of the Streets and Highways Code.
 2. **REVOCAION:** Encroachment permits are revocable on five (5) business days' notice unless otherwise stated on the permit and except as provided by law for public corporations, franchise holders, and utilities. Notwithstanding the foregoing, in an emergency situation as determined by the Department, an encroachment permit may be revoked immediately. These General Provisions and any applicable Special Provisions are subject to modification or abrogation by the Department at any time. Permittees' joint use agreements, franchise rights, reserved rights or any other agreements for operating purposes in State of California ("State") highway right-of-way may be exceptions to this revocation.
 3. **DENIAL FOR NONPAYMENT OF FEES:** Failure to pay encroachment permit fees when due may result in rejection of future applications and denial of encroachment permits.
 4. **ASSIGNMENT:** This encroachment permit allows only the Permittee or Permittee's authorized agent to work within or encroach upon the State Highway System, and the Permittee may not assign this permit.
 5. **ACCEPTANCE OF PROVISIONS:** Permittee understands and agrees to accept and comply with these General Provisions, the Special Provisions, any and all terms and/or conditions contained in or incorporated into the encroachment permit, and all attachments to the encroachment permit (collectively "the Permit Conditions"), for any encroachment, work, and/or activity to be performed under this encroachment permit and/or under color of authority of this encroachment permit. Permittee understands and agrees the Permit Conditions are applicable to and enforceable against Permittee as long as the encroachment remains in, under, or over any part of the State Highway System.
 6. **BEGINNING OF WORK:** When traffic is not impacted (see General Provision Number 35), the Permittee must notify the Department's representative two (2) business days before starting permitted work. Permittee must notify the Department's representative if the work is to be interrupted for a period of five (5) business days or more, unless otherwise agreed upon. All work must be performed on weekdays during regular work hours, excluding holidays, unless otherwise specified in this encroachment permit.
 7. **STANDARDS OF CONSTRUCTION:** All work performed within State highway right-of-way must conform to all applicable Departmental construction standards including but not limited to: Standard Specifications, Standard Plans, Project Development Procedures Manual, Highway Design Manual and Special Provisions.
- Other than as expressly provided by these General Provisions, the Special Provisions, the Standard Specifications, Standard Plans, and other applicable Departmental standards, nothing in these General Provisions is intended to give any third party any legal or equitable right, remedy, or claim with respect to these General Provisions or any provision herein. These General Provisions are for the sole and exclusive benefit of the Permittee and the Department.
- Where reference is made in such standards to "Contractor" and "Engineer," these are amended to be read as "Permittee" and "Department's representative," respectively, for purposes of this encroachment permit.
8. **PLAN CHANGES:** Deviations from plans, specifications, and/or the Permit Conditions as defined in General Provision Number 5 are not allowed without prior approval from the Department's representative.
 9. **INSPECTION AND APPROVAL:** All work is subject to monitoring and inspection. Upon completion of work, Permittee must request a final inspection for acceptance and approval by the Department. The local public agency Permittee must not give final construction approval to its contractor until final acceptance and approval by the Department is obtained.
 10. **PERMIT AT WORKSITE:** Permittee must keep the permit package or a copy thereof at the work site at all times and must show it upon request to any Department representative or law enforcement officer. If the permit package, or a copy thereof, is not kept and made available at the work site at all times, the work must be suspended.
 11. **CONFLICTING ENCROACHMENTS:** Permittee must yield start of work to ongoing, prior authorized work adjacent to or within the limits of the Permittee's project site. When existing encroachments conflict with Permittee's work, the Permittee must bear all cost for rearrangements (e.g., relocation, alteration, removal, etc.).
 12. **PERMITS FROM OTHER AGENCIES:** This encroachment permit is invalidated if the Permittee has not obtained all permits necessary and required by law, including but not limited to permits from the California Public Utilities Commission (CPUC), California Occupational Safety and Health Administration (Cal-OSHA), or any other public agency having jurisdiction. Permittee warrants all such permits have been obtained before beginning work under this encroachment permit.
 13. **PEDESTRIAN AND BICYCLIST SAFETY:** A safe minimum continuous passageway of four (4) feet must be maintained through the work area at existing pedestrian or bicycle facilities. At no time must pedestrians be diverted onto a portion of the street used for vehicular traffic. At locations where safe alternate passageways cannot be provided, appropriate signs and barricades must be installed at the limits of construction and in advance of the limits of construction at the nearest

- crosswalk or intersection to detour pedestrians to facilities across the street. Attention is directed to Section 7-1.04, Public Safety, of the Department's Standard Specifications.
14. **PUBLIC TRAFFIC CONTROL:** As required by law, the Permittee must provide traffic control protection, warning signs, lights, safety devices, etc., and take all other measures necessary for the traveling public's safety. While providing traffic control, the needs of all road users, including but not limited to motorists, bicyclists and pedestrians, including persons with disabilities in accordance with the Americans with Disabilities Act, must be an essential part of the work activity. Lane and/or shoulder closures must comply with the Department's Standard Specifications and Standard Plans for traffic control systems, and with the applicable Special Provisions. Where issues are not addressed in the Standard Specifications, Standard Plans, and/or Special Provisions, the California Manual on Uniform Traffic Control Devices (Part 6, Temporary Traffic Control) must be followed.
 15. **MINIMUM INTERFERENCE WITH TRAFFIC:** Permittee must plan and conduct work so as to create the least possible inconvenience to the traveling public, such that traffic is not unreasonably delayed.
 16. **STORAGE OF EQUIPMENT AND MATERIALS:** The storage of equipment or materials is not allowed within State highway right-of-way, unless specified within the Special Provisions of this encroachment permit. If encroachment permit Special Provisions allow for the storage of equipment or materials within the State highway right-of-way, the equipment and material storage must also comply with Section 7-1.04, Public Safety, of the Department's Standard Specifications.
 17. **CARE OF DRAINAGE:** Permittee must provide alternate drainage for any work interfering with an existing drainage facility in compliance with the Department's Standard Specifications, Standard Plans, and/or as directed by the Department's representative.
 18. **RESTORATION AND REPAIRS IN STATE HIGHWAY RIGHT-OF-WAY:** Permittee is responsible for restoration and repair of State highway right-of-way resulting from permitted work (Streets and Highways Code, section 670 et seq.).
 19. **STATE HIGHWAY RIGHT-OF-WAY CLEAN UP:** Upon completion of work, Permittee must remove and dispose of all scraps, refuse, brush, timber, materials, etc. off the State highway right-of-way. The aesthetics of the highway must be as it was before work started or better.
 20. **COST OF WORK:** Unless stated otherwise in the encroachment permit or a separate written agreement with the Department, the Permittee must bear all costs incurred for work within the State highway right-of-way and waives all claims for indemnification or contribution from the State, the Department, and from the Directors, officers, and employees of the State and/or the Department.
 21. **ACTUAL COST BILLING:** When specified in the permit, the Department will bill the Permittee actual costs at the currently set Standard Hourly Rate for encroachment permits.
 22. **AS-BUILT PLANS:** When required, Permittee must submit one (1) set of folded as-built plans within thirty (30) calendar days after completion and acceptance of work in compliance with requirements listed as follows:
 - a) Upon completion of the work provided herein, the Permittee must submit a paper set of As-Built plans to the Department's representative.
 - b) All changes in the work will be shown on the plans, as issued with the permit, including changes approved by Encroachment Permit Rider.
 - c) The plans are to be prominently stamped or otherwise noted "AS-BUILT" by the Permittee's representative who was responsible for overseeing the work. Any original plan that was approved with a Department stamp, or by signature of the Department's representative, must be used for producing the As-Built plans.
 - d) If construction plans include signing or striping, the dates of signing or striping removal, relocation, or installation must be shown on the As-Built plans when required as a condition of the encroachment permit. When the construction plans show signing and striping for staged construction on separate sheets, the sheet for each stage must show the removal, relocation, and installation dates of the appropriate staged striping and signing.
 - e) As-Built plans must contain the Encroachment Permit Number, County, Route, and Post Mile on each sheet.
 - f) The As-Built Plans must not include a disclaimer statement of any kind that differs from the obligations and protections provided by sections 6735 through 6735.6 of the California Business and Professions Code. Such statements constitute non-compliance with Encroachment Permit requirements and may result in the Department retaining Performance Bonds or deposits until proper plans are submitted. Failure to comply may also result in denial of future encroachment permits or a provision requiring a public agency to supply additional bonding.
 23. **PERMITS FOR RECORD PURPOSES ONLY:** When work in the State highway right-of-way is within an area under a Joint Use Agreement (JUA) or a Consent to Common Use Agreement (CCUA), a fee exempt encroachment permit is issued to the Permittee for the purpose of providing a notice and record of work. The Permittee's prior rights must be preserved without the intention of creating new or different rights or obligations. "Notice and Record Purposes Only" must be stamped across the face of the encroachment permit.
 24. **BONDING:** The Permittee must file bond(s), in advance, in the amount(s) set by the Department and using forms acceptable to the Department. The bonds must name the

Department as obligee. Failure to maintain bond(s) in full force and effect will result in the Department stopping all work under this encroachment permit and possibly revoking other encroachment permit(s). Bonds are not required of public corporations or privately-owned utilities unless Permittee failed to comply with the provisions and/or conditions of a prior encroachment permit. The surety company is responsible for any latent defects as provided in California Code of Civil Procedure section 337.15. A local public agency Permittee also must comply with the following requirements:

- a) In recognition that project construction work done on State property will not be directly funded and paid by State, for the purpose of protecting stop notice claimants and the interests of State relative to successful project completion, the local public agency Permittee agrees to require the construction contractor to furnish both a payment and performance bond in the local public agency's name with both bonds complying with the requirements set forth in Section 3-1.05 Contract Bonds of the Department's Standard Specifications before performing any project construction work.
 - b) The local public agency Permittee must defend, indemnify, and hold harmless the State and the Department, and the Directors, officers, employees, and agents of the State and/or Department, from all project construction related claims by contractors, subcontractors, and suppliers, and from all stop notice and/or mechanic's lien claimants. The local public agency also agrees to remedy, in a timely manner and to the Department's satisfaction, any latent defects occurring as a result of the project construction work.
25. **FUTURE MOVING OF INSTALLATIONS:** Permittee understands and agrees to relocate a permitted installation upon notice by the Department. Unless under prior property right or agreement, the Permittee must comply with said notice at the Permittee's sole expense.
26. **ENVIRONMENTAL:**
- a) **ARCHAEOLOGICAL/HISTORICAL:** If any archaeological or historical resources are identified or encountered in the work vicinity, the Permittee must immediately stop work, notify the Department's representative, retain a qualified archaeologist who must evaluate the site at Permittee's expense, and make recommendations to the Department's representative regarding the continuance of work.
 - b) **HAZARDOUS MATERIALS:** If any hazardous waste or materials (such as underground storage tanks, asbestos pipes, contaminated soil, etc.) are identified or encountered in the work vicinity, the Permittee must immediately stop work, notify the Department's representative, retain a qualified hazardous waste/material specialist who must evaluate the site at Permittee's expense, and make recommendations

to the Department's representative regarding the continuance of work.

Attention is directed to potential aerially deposited lead (ADL) presence in unpaved areas along highways. It is the Permittee's responsibility to take all appropriate measures to protect workers in conformance with California Code of Regulations Title 8, Section 1532.1, "Lead," and with Cal-OSHA Construction Safety Orders, and to ensure roadway soil management is in compliance with Department of Toxic Substances Control (DTSC) requirements.

27. **PREVAILING WAGES:** Work performed by or under an encroachment permit may require Permittee's contractors and subcontractors to pay appropriate prevailing wages as set by the California Department of Industrial Relations. Inquiries or requests for interpretations relative to enforcement of prevailing wage requirements must be directed to the California Department of Industrial Relations.
28. **LIABILITY, DEFENSE, AND INDEMNITY:** The Permittee agrees to indemnify and save harmless the State, the Department, and the Directors, officers, employees, agents and/or contractors of the State and/or of the Department, including but not limited to the Director of Transportation and the Deputy Directors, from any and all claims, demands, damages, costs, liability, suits, or actions of every name, kind, and description, including but not limited to those brought for or on account of property damage, invasion of privacy, violation or deprivation of a right under a state or federal law, environmental damage or penalty, or injury to or death of any person including but not limited to members of the public, the Permittee, persons employed by the Permittee, and/or persons acting on behalf of the Permittee, arising out of or in connection with: (a) the issuance and/or use of this encroachment permit; and/or (b) the encroachment, work, and/or activity conducted pursuant to this encroachment permit, or under color of authority of this encroachment permit but not in full compliance with the Permit Conditions as defined in General Provision Number 5 ("Unauthorized Work or Activity"); and/or (c) the installation, placement, design, existence, operation, and/or maintenance of the encroachment, work, and/or activity; and/or (d) the failure by the Permittee or anyone acting on behalf of the Permittee to perform the Permittee's obligations under any part of the Permit Conditions as defined in General Provision Number 5, in respect to maintenance or any other obligation; and/or (e) any change to the Department's property or adjacent property, including but not limited to the features or conditions of either of them, made by the Permittee or anyone acting on behalf of the Permittee; and/or (f) a defect or obstruction related to or caused by the encroachment, work, and/or activity whether conducted in compliance with the Permit Conditions as defined in General Provision Number 5 or constituting Unauthorized Work or Activity, or from any cause whatsoever. The duty

of the Permittee to indemnify and save harmless includes the duties to defend as set forth in Section 2778 of the Civil Code.

It is the intent of the parties that except as prohibited by law, the Permittee will defend, indemnify, and hold harmless as set forth in this General Provision Number 28 regardless of the existence or degree of fault or negligence, whether active or passive, primary or secondary, on the part of: the State; the Department; the Directors, officers, employees, agents and/or contractors of the State and/or of the Department, including but not limited to the Director of Transportation and the Deputy Directors; the Permittee; persons employed by the Permittee; and/or persons acting on behalf of the Permittee.

The Permittee waives any and all rights to any type of expressed or implied indemnity from or against the State, the Department, and the Directors, officers, employees, agents, and/or contractors of the State and/or of the Department, including but not limited to the Director of Transportation and the Deputy Directors.

The Permittee understands and agrees to comply with the obligations of Titles II and III of the Americans with Disabilities Act in the conduct of the encroachment, work, and/or activity whether conducted pursuant to this encroachment permit or constituting Unauthorized Work or Activity, and further agrees to defend, indemnify, and save harmless the State, the Department, and the Directors, officers, employees, agents, and/or contractors of the State and/or of the Department, including but not limited to the Director of Transportation and the Deputy Directors, from any and all claims, demands, damages, costs, penalties, liability, suits, or actions of every name, kind, and description arising out of or by virtue of the Americans with Disabilities Act.

The Permittee understands and agrees the Directors, officers, employees, agents, and/or contractors of the State and/or of the Department, including but not limited to the Director of Transportation and the Deputy Directors, are not personally responsible for any liability arising from or by virtue of this encroachment permit.

For the purpose of this General Provision Number 28 and all paragraphs herein, "contractors of the State and/or of the Department" includes contractors and their subcontractors under contract to the State and/or the Department.

This General Provision Number 28 and all paragraphs herein take effect immediately upon issuance of this encroachment permit, and apply before, during, and after the encroachment, work, and/or activity contemplated under this encroachment permit, whether such work is in compliance with the Permit Conditions as defined in General Provision Number 5 or constitutes Unauthorized Work or Activity, except as otherwise provided by California law. The Permittee's obligations to defend, indemnify, and save harmless under this General Provision Number 28 take effect immediately upon

issuance of this encroachment permit and have no expiration date, including but not limited to situations in which this encroachment permit expires or is revoked, the work or activity performed under this encroachment permit is accepted or not accepted by the Department, the encroachment, work, and/or activity is conducted in compliance with the Permit Conditions as defined in General Provision Number 5 or constitutes Unauthorized Work or Activity, and/or no work or activity is undertaken by the Permittee or by others on the Permittee's behalf.

29. **NO PRECEDENT ESTABLISHED:** This encroachment permit is issued with the understanding that it does not establish a precedent.

30. **FEDERAL CIVIL RIGHTS REQUIREMENTS FOR PUBLIC ACCOMMODATION:**

a) As part of the consideration for being issued this encroachment permit, the Permittee, on behalf of Permittee and on behalf of Permittee's personal representatives, successors in interest, and assigns, does hereby covenant and agree that:

i) No person on the grounds of race, color, or national origin may be excluded from participation in, be denied the benefits of, or be otherwise subjected to discrimination in the use of said facilities.

ii) That in connection with the construction of any improvements on said lands and the furnishings of services thereon, no discrimination must be practiced in the selection and retention of first-tier subcontractors in the selection of second-tier subcontractors.

iii) That such discrimination must not be practiced against the public in their access to and use of the facilities and services provided for public accommodations (such as eating, sleeping, rest, recreation), and operation on, over, or under the space of the State highway right-of-way.

iv) That the Permittee must use the premises in compliance with all other requirements imposed pursuant to Title 15, Code of Federal Regulations, Commerce and Foreign Trade, Subtitle A. Office of the Secretary of Commerce, Part 8 (15 C.F.R. Part 8) and as said Regulations may be amended.

b) That in the event of breach of any of the above nondiscrimination covenants, the State and the Department have the right to terminate this encroachment permit and to re-enter and repossess said land and the facilities thereon and hold the same as if said permit had never been made or issued.

31. **MAINTENANCE OF HIGHWAYS:** By accepting this encroachment permit, the Permittee agrees to properly maintain any encroachment. This assurance requires the Permittee to provide inspection and repair any damage, at Permittee's expense, to State facilities resulting from the encroachment.

32. **SPECIAL EVENTS:** In accordance with subdivision (a) of Streets and Highways Code section 682.5 and 682.7, the Department is not responsible for the conduct or operation of the permitted activity, and the applicant agrees to defend, indemnify, and hold harmless the State, the Department, and the Directors, officers, employees, agents, and contractors of the State and/or of the Department, including but not limited to the Director of Transportation and the Deputy Directors, from any and all claims, demands, damages, costs, liability, suits, or actions of every name, kind and description arising out of any activity for which this encroachment permit is issued. The Permittee is required, as a condition of this encroachment permit, for any event that awards prize compensation to competitors in gendered categories, for any participant level that receives prize compensation, to ensure the prize compensation for each gendered category is identical at each participant level. (Streets and Highways Code, section 682.7.)
The Permittee understands and agrees to comply with the obligations of Titles II and III of the Americans with Disabilities Act in the conduct of the event, and further agrees to defend, indemnify, and save harmless the State and the Department, and the Directors, officers, and employees of the State and/or Department, including but not limited to the Director of the Department and the Deputy Directors, from any and all claims, demands, damages, costs, liability, suits, or actions of every name, kind and description arising out of or by virtue of the Americans with Disabilities Act.
33. **PRIVATE USE OF STATE HIGHWAY RIGHT-OF-WAY:** State highway right-of-way must not be used for private purposes without compensation to the State. The gifting of public property uses and therefore public funds is prohibited under the California Constitution, Article XVI, Section 6.
34. **FIELD WORK REIMBURSEMENT:** Permittee must reimburse the Department for field work performed on Permittee's behalf to correct or remedy hazards or damaged facilities, or to clear refuse, debris, etc. not attended to by the Permittee.
35. **LANE CLOSURE REQUEST SUBMITTALS AND NOTIFICATION OF CLOSURES TO THE DEPARTMENT:** Attention is directed to Section 12-4.02A(3) Submittals, of the Department's Standard Specifications, for lane closure requests submittals requirements and schedules. The Permittee must notify the Department's representative and the Traffic Management Center (TMC) before initiating a lane closure or conducting an activity that may cause a traffic impact. In emergency situations when the corrective work or the emergency itself may affect traffic, the Department's representative and the TMC must be notified as soon as possible.
36. **SUSPENSION OF TRAFFIC CONTROL OPERATION:** The Permittee, upon notification by the Department's representative, must immediately suspend all lane closure operations and any operation that impedes the flow of traffic. All costs associated with this suspension must be borne by the Permittee.
37. **UNDERGROUND SERVICE ALERT (USA) NOTIFICATION:** Any excavation requires compliance with the provisions of Government Code section 4216 et. seq., including but not limited to notice to a regional notification center, such as Underground Service Alert (USA). The Permittee must provide notification to the regional notification center at least forty-eight (48) hours before performing any excavation work within the State highway right-of-way.
38. **COMPLIANCE WITH THE AMERICANS WITH DISABILITIES ACT (ADA):** All work within the State highway right-of-way to construct and/or maintain any public facility must be designed, maintained, and constructed strictly in accordance with all applicable Federal Access laws and regulations (including but not limited to Section 504 of the Rehabilitation Act of 1973, codified at 29 U.S.C. § 794), California Access laws and regulations relating to ADA, along with its implementing regulations, Title 28 of the Code of Federal Regulations Parts 35 and 36 (28 C.F.R., Ch. I, Part 35, § 35.101 et seq., and Part 36, § 36.101 et seq.), Title 36 of the Code of Federal Regulations Part 1191 (36 C.F.R., Ch. XI, Part 1191, § 1191.1 et seq.), Title 49 of the Code of Federal Regulations Part 37 (49 C.F.R., Ch. A, Part 37, § 37.1 et seq.), the United States Department of Justice Title II and Title III for the ADA, and California Government Code section 4450 et seq., which require public facilities be made accessible to persons with disabilities.
Notwithstanding the requirements of the previous paragraph, all construction, design, and maintenance of public facilities must also comply with the Department's Design Information Bulletin 82, "Pedestrian Accessibility Guidelines for Highway Projects."
39. **STORMWATER:** The Permittee is responsible for full compliance with the following:
- For all projects, the Department's Storm Water Program and the Department's National Pollutant Discharge Elimination System (NPDES) Permit requirements under Order No. 2012-0011-DWQ, NPDES No CAS000003; and
 - In addition, for projects disturbing one acre or more of soil, with the California Construction General Permit Order No. 2009-0009-DWQ, NPDES No CAS000002; and
 - In addition, for projects disturbing one acre or more of soil in the Lahontan Region with Order No. R6T-2016-0010, NPDES No CAG616002.
 - For all projects, it is the Permittee's responsibility to install, inspect, repair, and maintain all facilities and devices used for water pollution control practices (Best Management Practices/BMPs) before performing daily work activities.

1. GENERAL: The purpose of these Special Provisions is to provide the Permittee with specifications for water pollution control to minimize, prevent, or control the discharge of material into the air, surface waters, groundwater, and storm sewers owned by the State or local agencies. These provisions are not intended to take the place of the Caltrans Water Pollution Control Program (WPCP) for projects where soil disturbance from work activities less than one acre, or work activities of one acre or more subject to the preparation of the Caltrans Storm Water Pollution Prevention Plan (SWPPP). The Permittee must comply with the following Special Provisions and the direction of the State Representative. All Stormwater Best Management Practices (BMPs) must conform to Section 13 Water Pollution Control of Caltrans' Standard Specifications.

2. NPDES REQUIREMENTS: The Permittee must be responsible for full compliance with the Caltrans Storm Water Program and the Caltrans National Pollutant Discharge Elimination System (NPDES) Permit requirements (*Order No. 2012-0011-DWQ, NPDES No CAS000003*) and for and projects disturbing one acre or more of soil, full compliance with the California Construction General Permit (*Order No. 2009-0009-DWQ, NPDES No CAS000002*) or for projects for projects that have one acre or more of soil disturbance in the Lahontan Region (*Order No. R6T-2016-0010, NPDES No CAG616002*). It is the Permittee's responsibility to install, inspect, and repair or maintain facilities and devices used for water pollution control practices (BMPs) before performing daily work activities. Installation, inspection and maintenance responsibilities on the job site include: 1) soil stabilization materials in work areas that are inactive or prior to storm events, 2) water pollution control devices to control sediment and erosion, 3) implementation of spill and leak prevention procedures for chemical and hazardous substances stored on the job site, 4) material storage, 5) stockpile management, 6) waste management, 7) non-stormwater management, 8) water conservation, 9) tracking controls and 10) illicit connection, illegal discharge detection and reporting. The Permittee must report to the State representative when discharges enter into receiving waters, adjacent property, drainage systems or when discharges could be a cause or a threat for water pollution. The Permittee must also control illicit discharges or illegal dumping prior to start of daily work schedule. Copies of written notices or orders from the Regional Water Quality Control Board or other regulatory agency must be provided to the State representative within 48 hours of reported activity. For additional information on stormwater compliance, visit the State Water Resources Control Boards storm water Website at:

http://www.waterboards.ca.gov/water_issues/programs/stormwater

3. RESPONSIBILITY FOR DEBRIS REMOVAL: The Permittee must be responsible for preventing sediment, trash, debris, and other construction waste from entering the street, the storm drains, local creeks, or any other bodies of water.

4. SPOILS AND RESIDUE: The Permittee must vacuum any saw-cut concrete waste material, debris, residue, etc. No spoils, debris, residue, etc. must be washed into a drainage system.

5. SWEEPING: Sweep paved roads at construction entrance and exit locations and surrounding paved areas daily within the job site during: 1) clearing and grubbing, 2) earthwork, 3) trenching, 4) soil disturbance, 5) pavement grinding and/or cutting, and 6) after observing tracking of material onto or off the State property. Keep dust to a minimum during sweeping activities. Use vacuum whenever dust generation is excessive or sediment pickup is ineffective.

Roadways or work areas must not be washed down with water. Street sweeping operations must conform to Section 13 Water Pollution Control of Caltrans' Standard Specifications.

6. VEHICLES AND EQUIPMENT: Permittee must prevent all vehicles, equipment, etc. from leakage or mud tracking onto roadways. If leaks cannot be repaired immediately, remove the vehicle or equipment from the job site.

7. MAINTENANCE AND FUELING OF VEHICLES AND EQUIPMENT: Maintenance and fueling of equipment must not result in any pollution at the job site. The Permittee must immediately clean up spills/leaks, and properly dispose of contaminated soil and materials.

8. CLEANING VEHICLES AND EQUIPMENT: Limit vehicle and equipment cleaning or washing at the job site except what is necessary to control vehicle tracking or hazardous waste. The Permittee must clean all equipment within a bermed area or over a drip pan large enough to prevent run-off. No soaps, solvents, degreasers, etc. must be used in State right-of-way. Any water from this operation must be collected and disposed of at an appropriate site. Containment berms or dikes must be used for fueling, washing, maintaining and washing vehicles or equipment in outside areas. Containment must be performed at least 100 feet from concentrated flows of storm water, drainage courses, and storm drain inlets if within a flood plain, otherwise at least 50 feet if outside the floodplain. Keep adequate quantities of absorbent spill- cleanup material and spill kits in the fueling or maintenance area and on fueling trucks.

9. DIESEL FUELS: The use of diesel fuel from petroleum or other fossil fuel as a form-oil or solvent is not allowed.

10. WEATHER CONDITIONS AT WORKSITE: Any activity that would generate fine particles or dust that could be transported off site by stormwater must be performed during dry weather.

11. WIND EROSION PROTECTION: The use of Wind Erosion BMPs must be deployed year-round in instances where dust or fine particles could be transported off site.

11. HOT MIX ASPHALT: Runoff from washing hot mix asphalt must not enter into any drainage conveyances.

12. PROTECTION OF DRAINAGE FACILITIES: The Permittee must protect/cover gutters, ditches, drainage courses, and inlets with gravel bags, fiber rolls, State approved fabric filters, etc., to the satisfaction of the State representative during grading, paving, saw-cutting, etc. and materials must conform to Section 13-6.02 Materials for Water Pollution Control of Caltrans' Standard Specifications. No such protection measures must cause an obstruction to the traveling public. The Permittee must implement spill and leak prevention procedures for chemicals and hazardous substances stored on the job site (including secondary containment requirements) in accordance to section 13-4.03B Spill Prevention and Control, and 14-11 Hazardous Waste and Contamination, Water Pollution Control of Caltrans' Standard Specifications.

13. PAINT: Rinsing of painting equipment and materials is not permitted in State right-of-way. When thoroughly dry, dispose of the following as solid waste: dry latex paint, paint cans, used brushes, rags, gloves, absorbent materials, and drop cloths. Oil based paint sludge and unusable thinner must be disposed of at an approved hazardous waste site.

14. CONSTRUCTION MATERIALS: Stockpile of all construction materials, including, but not limited to; pressure treated wood, asphalt concrete, cold mix asphalt concrete, concrete, grout, cement containing premixes, and mortar, must conform to section 13-4.03C (2) Material Storage & 13-4.03C (3) Stockpile Management of Caltrans' Standard Specifications.

15. CONCRETE EQUIPMENT: Concrete equipment must be washed in a designated washing area in a way that does not contaminate soil, receiving waters, or storm drain systems.

16. EXISTING VEGETATION: Established existing vegetation is the best form of erosion control. Minimize disturbance to existing vegetation. Damaged or removed vegetation must be replaced as directed by the State Representative.

17. SOIL DISTURBANCE: Soil disturbing activities must be avoided during the wet weather season. If construction activities during wet weather are allowed in your permit, all necessary erosion control and soil stabilization measures must be implemented in advance of soil disturbing activity.

18. SLOPE STABILIZATION AND SEDIMENT CONTROL: Consider a certified expert in Erosion and Sediment control in cases where slopes are disturbed during construction. The Permittee is directed to comply with Section 13.5 Temporary Soil Stabilization and Section 21 Erosion Control of Caltrans' Standard Specifications during application of temporary soil stabilization measures to the soil surface. Fiber rolls or silt fences may be required down slope until permanent soil stabilization is established. Remove the accumulated sediment whenever the sediment accumulates to 1/3 of the linear sediment barrier height. The Permittee must limit the use of plastic materials when more sustainable, environmentally friendly alternatives exist or when environmental regulations prohibit their use within the project.

19. STOCKPILES: Stockpiles containing aggregate and/or soil must be stored at least 100 feet from concentrated flows of storm water, drainage courses, and storm drain inlets if within a flood plain, otherwise at least 50 feet if outside the floodplain, and must be covered and protected with a temporary perimeter sediment barrier. Cold mix stockpiles must be stored on an impermeable surface and covered with 9 mil plastic to prevent contact with water. Minimize stockpiling of materials on the job site. Manage stockpiles by implementing the water pollution control practices in Section 13-4.03C (3) Stockpile Management of the State of California standard specifications for construction.

20. DISCOVERY OF CONTAMINATION: The State Representative must be notified in case any unusual discoloration, odor, or texture of ground water, is found in excavated material or if abandoned, underground tanks, pipes, or buried debris are encountered.

21. SANITARY AND SEPTIC WASTE: Do not bury or discharge wastewater from a sanitary or septic system within the highway. Properly connected sewer facilities are free from leaks. With State Representative approval place portable sanitary facility at least 50 feet away from storm drains, receiving waters, and flow lines. Permittee must comply with local health agency provisions when using an on-site disposal system.

22. LIQUID WASTE: Prevent job site liquid waste from entering storm drain systems and receiving waters. Drilling slurries, grease or oil-free waste water or rinse water, dredging, wash water or rinse water running off a surface or other non-storm water liquids not covered under separate waste water permits must be held in structurally sound, leak-proof containers, such as portable bins or portable tanks. Store containers at least 50 feet away from moving vehicles and equipment. Liquid waste may require testing to determine hazardous material content prior to disposal. All measures must conform to section 13-4.03D (5) Liquid Waste, Water Pollution Control of Caltrans' Standard Specifications.

23. WATER CONTROL AND CONSERVATION: Manage water use in a way that will prevent erosion and the discharge of

pollutants into storm drain systems and receiving waters. Direct runoff, including water from water line repair from the job site to areas where it can infiltrate into the ground. Direct water from off-site sources around the job site or from contact with jobsite runoff.

24. PILE DRIVING: Keep spill kits and cleanup materials at pile driving locations. Park pile driving equipment over drip pans, absorbent pads, or plastic sheeting with absorbent material, and away from stormwater run-on when not in use.

25. DEWATERING: Dewatering consists of discharging accumulated storm water, groundwater, or surface water from excavations or temporary containment facilities. All dewatering operations must comply with the latest Caltrans guidelines including the *Field Guide for Construction Site Dewatering*. Contact State representative for approval of dewatering discharge by infiltration or evaporation, otherwise, any effluent discharged into a permitted storm water system requires approval from the Regional Water Quality Control Board. Prior to the start of dewatering, the Permittee must provide the State Representative with a dewatering and discharge work plan that complies with section 13-4.03G Dewatering, Water Pollution Control of Caltrans' Standard Specifications. A copy of the Waste Discharge Permit and a copy of a valid WDID number issued by the Regional Board must be provided to the State representative.



City of Sebastopol Temporary Parklet Program Survey Results

From May 7, 2021 to May 28, 2021, the City of Sebastopol conducted a survey to determine public sentiment about continuing the temporary approval of parklets throughout the survey. The survey was promoted through paid and organic promotion on the city’s Facebook page, NextDoor, and Twitter as well as via the Townsy newsletter and app. It was also distributed to all businesses, many of which posted the survey at their locations and in the parklets themselves.

Take the Sebastopol Temporary Parklet Program Survey!

In an effort to better serve our community during the pandemic, Sebastopol applied for and received permission from Caltrans (which is responsible for Highways 12 and 116) for the temporary installation of several parklets (outdoor seating areas adjacent to the sidewalks in the parking lane of the street) in the downtown area.

Parklets are considered public spaces and are not reserved for patrons of adjacent businesses. Businesses commit to keeping the parklets in front of or near their establishments clean, but the parklets may be used by anyone.

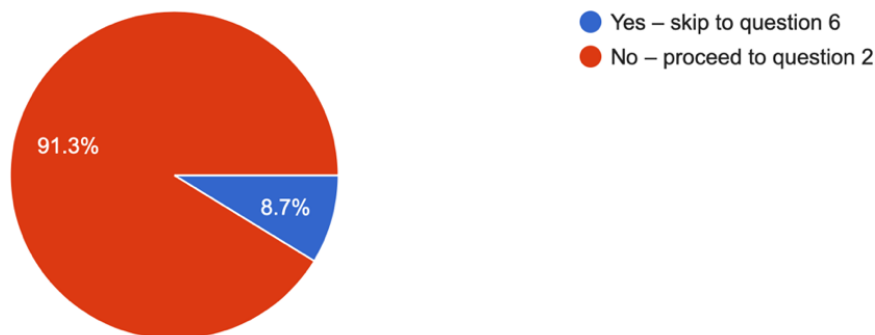
We would like to hear from community members about whether or not they would like the parklets to continue.

Survey results will be shared with the City Council to help inform their decision-making process about the future of parklets in Downtown Sebastopol.

583 responses

1) Are you a Downtown Sebastopol business owner?

583 responses





If you selected f. Other above please describe.

39 responses

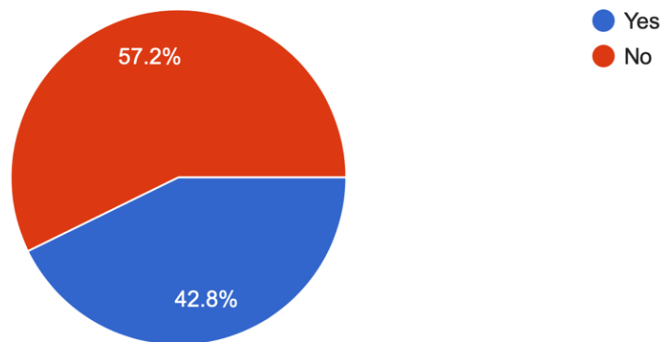
1. Really Car pollution Is So Unappeting
2. Thought they were related only to the establishment they were in front of. Did not realize that they were open to anyone as a public space
3. Sunshine Cafe has better seating on sidewalks
4. Without a selection in question 4, the form submit button does not appear.
5. Until reading about them before taking this I thought it was reserved for patrons of the business it was in front of. I sat in one once to rest while shopping but I was afraid I would be thrown out .
6. Why sit when vehicle exhaust pipes are spewing all over you and your food. I can't believe anyone would want to experience this. Yuk!!
7. Only just started being out in public more after sheltering.
8. Parking in Sebastopol is not easy y'all keep getting rid of parking
9. I wouldn't eat next to traffic and traffic fumes.
10. Too sunny and no shade
11. I thought it was connected to the business close to it
12. Do not find the space appealing.
13. I don't like to be near moving traffic and auto exhaust.
14. We haven't been dining out yet.
15. Don't want to eat next to traffic.
16. I am happy to have the option in seating for restaurants, but I don't shop downtown because stores don't open till 11:00. and close at 5:00. I'm off to a different town that is open!
17. Too many people using at the time of my arrival on getting my food
18. Test answer
19. Too small to accommodate more than just a few people
20. I can never find parking, so most of my shopping is done out of town.
21. Exhaust from vehicles and noise made it unappealing
22. Covid sheltered home but now going out.
23. Do not go out! Don't want to be around people!
24. Just haven't ventured there yet
25. Sitting along a busy street is not an appealing place for a meal.
26. They look like shit and 2 of them were in the wrong place. You should have researched other cities parklets (SF, Cotati, etc.)
27. The ones on the street are loud due to traffic, especially those on Main Street. Not inviting.
28. I have not eaten out.
29. Just haven't been out that much yet, and not hanging out downtown yet.
30. Doesn't seem like a very pleasant place to sit, so close to traffic.
31. The only ones I've seen are on Main St. and it's too noisy there.
32. Traffic noise and fumes
33. I'm handicapped and old . Main St. offers no parking for the handicapped, so I avoid ALL downtown biz and activities.
34. We plan to!
35. They feel unsafe both due to traffic and Covid.



- 36. N/a was not an option
- 37. I have
- 38. Wanted to use yet people without masks, so did not
- 39. I don't do much downtown and haven't gone to lunch or dinner downtown for years.

5) Has knowing that there are parklets available impacted your decision to visit the downtown area?

545 responses



5a) If you wish, use this space to tell us more about how knowing that there are parklets available impacted your decision to visit the downtown area.

188 responses

1. I appreciate that there is more outside seating. It does take parking spaces though
2. easy in/out for pick up food
3. I don't have much reason to be in downtown Sebastopol
4. Having more space to sit makes me more likely to spend time downtown
5. I have not been out very much during the pandemic other than grocery shopping and occasional walks at Ragle, Joe Rodota Trail, etc. Now that I'm vaccinated and our SoCo infection rate is declining, I intend to get out more. I have been aware of the parklets and look forward to using them.
6. I haven't had the money to eat out but if I did I would want to eat outside. So access to the park let's for outdoor dining will impact whether I feel safe eating at a restaurant. I'm not eating indoors.
7. I love having more outdoor space to come together in community with our neighbors
8. Test answer
9. This makes the downtown more attractive than just as a highway. Check out downtown Mt view Castro for a vision of what this might become
10. I appreciate being able to sit outside
11. Only use Screaming Mimi's.
12. Love the outdoor space
13. Love downtown and feels safer to eat outside.
14. Like to eat outside and it's a great buffer from traffic.



15. More parklets are better! Thanks!
16. I've been limiting my visits outside the house due to covid.
17. 1) You favored some business and not others. Ginger Thai didn't get one.
2) I don't feel comfortable walking past them because of potential exposure to the virus. So, I don't walk around downtown.
3) It limits available parking.
4) Get rid of them along with the bike lanes
18. Decreased parking available
19. I wanted to enjoy a coffee out but there was no where to sit otherwise except a park bench which is often taken. The parklets are roomy.
20. Didn't make any difference. Once we are OK with going to restaurants again we will eat inside anyway
21. I know they are available and go downtown to use them and to support the local businesses.
22. Knowing there was space set aside for folks to enjoy food/drinks from downtown establishments while staying off the sidewalk (and thus keeping downtown walkable and keeping others a safe distance away from maskless eaters/drinkers) was comforting.
23. They are great!
24. I think they are great. Love the one in front of Screamin Mimi.
25. felt comfortable going more often
26. They haven't. Last thing I need is a car accident to go through that tiny ass wall and hospitalize me from either the wall or a vehicle.

Take away more parking and through roads causing more traffic sounds like a good idea.

27. Able to be outside and feel safer during Covid.
28. Even though I haven't used them yet, I plan on using one soon and I enjoy seeing people using them when I am downtown.
29. Happy to sit down with people I run into downtown
30. They make the parking situation worse and look unattractive for this small little quaint town.
31. I enjoy eating outside and they make our town seem friendlier and more welcoming.
32. Our family really enjoys the park let's because it makes it safe for us to frequent some of the downtown businesses while maintaining a Covid safe distance from other people.
33. It has reduced parking downtown.
34. It makes me want to spend more time downtown. I love them!!! It helps it to feel less like we have Hwys intersecting downtown.
35. Outdoor seating
36. I love parklets! It makes downtown feel more vital. I wish there were more!
37. The parklets make it safer to visit downtown businesses, and also provide a sense of community without impacting the flow of traffic. They're great!
38. There are some places the park let's make a lot of sense, such as in front of screaming mimis where that silly bit of road causes more traffic issues than it alleviates. Other places, the park let's feel too forced, cramped and uninviting.
39. Knowing There Is No Publicly Announced Local City Health Order Leaves Me In Despair For The Future Of Our City Employees Futures
40. I see that people are supporting restaurants and eating at the park let's. I think they should continue until we reach the yellow level.
41. More seating for lunches, etc.
42. It makes parking worse, social distancing impossible, and I choose to shop other places due to the crowding for cars & people.
43. Takes away parking.
44. I love the parklets and knowing they are there to provide extra outdoor space for the past months has made me feel safer visiting downtown. Make them permanent!



45. I thought after June 15 Mr. Newsom said he was going to o"open up everything in California. Except mask mandate would still be enforced. So it would seem to be a waste of resources all around.
46. Businesses I use don't have them
47. I live within walking distance of downtown. As the weather improves and covid subsides I would love to dine in parklets
48. Made it possible to sit and enjoy ice cream and breakfast at a different spot.
49. It's a very nice feeling eating out side in town. These parklets need to stay. It makes Sebastopol feel like a community that welcomes visitors.
50. I liked having a dedicated spot where my children could eat their ice cream that wasn't the town square as it was busy.
51. Too many people
52. Too crowded to safely walk on the sidewalk and stay 6 feet apart.
53. It's a waste of already limited downtown parking
54. Outdoor dining compels me to dine out more often.
55. feels safe to dine outside.
56. Less parking
57. they are great, so nice to have outside space near businesses with food and drinks. keep them please
58. Easy outdoor safe dining
59. In general, I have avoided public gatherings. However, I like the parklets and would like to see them as a permanent fixture.
60. Parklets provide a more-inviting downtown and offers additional seating options outside while not impeding adjacent sidewalks.
61. It didn't, only because I wasn't going to go downtown until I was fully vaccinated (which happened this week). But I love that they're there!
62. We need parking!
63. Less parking spots
64. The park let's made it easier to social distance which has been wonderful!
65. Easy to avoid if concerned about unmasked diners
66. I do not want to dine indoors, and knowing I could take my food someplace was a big help
67. Safety from Covid-19 & nice to be able to sit outside in the sunshine and be a part of the community spirit.
68. It has favorably impacted my decision in that I think they are GREAT and I am happy to be able to use parklets!
69. I like them. Even if just to chat with people I see in the street without taking up the sidewalk
70. The parklets have been an amazing addition to create more community space. They provide space for not only guests to safely dine/drink outside, but also for parents with strollers, bicycle parking (there is a huge lack of this in downtown so the additional bike parking has been welcomed) and for handicap folks in a wheelchair or otherwise. People enjoy coming to downtown more knowing there is a seat for them outside and additional space for them to safely gather.
71. I appreciate having a place outside to sit after I buy something, and to visit with friends.
72. I think they increase the comfort level of people eating out
73. The parklets provide a comfortable outdoor experience that would not be possible otherwise. I'd even be willing to have less sidewalk space available to have this experience integrated better with businesses.
74. Avoid. I saw people congregating too closely.
75. Love parkets in general but the ones in Sebastopol are unattractive, especially with the concrete barriers.
76. We advocate for more parking in the downtown area. Taking away parking spaces makes me less likely to park downtown and walk around.
Also, blocking the road in front of screaming Mimi's really makes traffic worse. Instead of looking at ways to make less traffic I feel that the city and the city Council is just finding ways to increase the amount of traffic in this town.
77. Parking was already limited (before Covid), now its very difficult to find parking. No more parklets Please!
78. No real impact either way
79. Perhaps a sign that indicates that they are "public space"
80. I walk downtown more often to get coffee or a bite to eat because I know there is a safe place to sit out if the way of foot traffic



81. For this pandemic, having more protected outdoor space, near businesses, is a great addition because I knew there were places to enjoy food outside while distanced.
82. There's a place for us to land while patronizing local stores and more importantly it makes the town more festive and alluring to walk around.
83. It's fun to know you might run into someone spontaneously. Especially during pandemic, nice to have outdoor gathering spots.
84. I went out to dinner
85. I like to know that there are additional outdoor seating areas to use when visiting some of my favorite business that do now have their own outdoor patios or seating areas. I think most of them take up like 2-3 parking spots but with the lots around town and other street parking I don't think it's an issue!
86. I'd much rather people in parcels than parked cars. They feel safe and welcoming. I just wish more businesses made use of them.
87. They look nice and make traffic less intense.
88. The one by screaming mimi's makes it much harder to leave depot Street. Blocking an entire driving street I think has made an inconvenience to those around Depot Street.
89. It feels friendly, a way to experience the town in a relaxed manner
90. I live just a two minute walk to downtown Sebastopol and I spent more time in the past 6 months in the downtown area than I have my entire 20 years of living here. The parklets made our downtown more pedestrian friendly and welcoming. I drink a coffee in the Retrograde parklet every week and honestly would not have ever gone there if it weren't for the parklet. Having the parklets made the downtown area genuinely enjoyable and homey. As someone living in Sebastopol in their 20s, I feel like the parklets have created space to people watch and bring friends into the Sebastopol landscape. I truly think those parklets (especially once things open up fully and more people populate the downtown area) have transformed the Sebastopol experience and I would be very upset to see them go.
91. The parklet for Screamin Mimis makes the most sense. I'd like to see this one continued, but the rest seem less safe.
92. I wanted to support our downtown restaurants and felt Covid safe doing so. I felt bad that East/West cafe didn't have a viable parklet. I would have gone there. Though I love the Gypsy Cafe their outdoor seating area did NOT feel safe and really hampered foot traffic on Main Street. I do hope that curbside pickup spaces at Copperfields goes away.
93. Certainly hasn't stopped me.
94. It makes it more fun and accessible during the pandemic but also in general so I can sit and enjoy my favorite places that may not have previously had outside seating. It's a major treat!
95. Having a place to sit outside of businesses is great
96. If I were walking downtown I might use one. If I got an ice cream cone I might use one. Just haven't been downtown for leisure.
97. It provides great gathering spaces or a brief sit-down before continuing shopping.
98. I like sitting outdoors downtown to eat and socialize
99. I go there more to eat outside, I like it.
100. Less parking available
101. I live downtown, and I think there a beautiful addition to Sebastopol. We need more life on our streets, rather than stinky cars. More places for people mingling is healthy and green.
102. Difficult to park shopping elsewhere
103. I think they are a GREAT addition to make downtown more inviting to shoppers.
104. Feel safe eating outside. Do not feel safe eating indoors. Will wait for two weeks of ZERO new covid infections before eating indoors.
105. TRAFFIC - noise, speed. smell... Sebastopol is a roadside service on the side of a major highway.
106. Parking in the downtown area is difficult. The parklets take too many spaces.
107. I love them. Now that summer is coming but you need to make them look nicer like the other towns. San Francisco a big city and theirs are really nice. All other towns are nicer. Ours are the dumps. Have some pride.
108. I would love to see more parklets - infact even pedestrian only downtown times
109. They are way to close to traffic, exhaust etc.
110. It feels nice to walk along streets where there is plenty of room for pedestrians and cafe patrons.
111. I don't like them at all
112. I love the new open air gathering space they provide
113. When we wanted to go out for ice cream, we were able to sit and enjoy it and be with other folks.



- 114. parking is at a premium- i'm not sure all of the parklet business are equally deserving vs them vs parking spaces.
- 115. I think they're important to the community and will give me the opportunity to stay downtown longer.
- 116. I want to visit ASAP!
- 117. I like having the parklets in place, they give downtown a relaxed and people friendly feel. Please keep them!
- 118. It made me want to spend more time enjoying downtown rather than just park and go
- 119. I like have space to enjoy my downtown treats outdoors in a safe area that's not just a sidewalk
- 120. It makes it nicer to visit downtown
- 121. This has hindered our business as there is no restaurant around it. It is in front of Sumbody and we have lost our walk in traffic due to it as well as customers complain that the piano music is too loud and leave the store. While I think they make sense to help restaurants it make no sense where this one is and no one is ever using it unless it is to play the piano they put in there. It is not a plus for any business
- 122. Safe outdoor space
- 123. Will hang out longer. Visit like a tourist in my own town after ice cream or a coffee
- 124. I enjoy the seating at the restaurants and the way they make me feel safer as a pedestrian
- 125. Made it safer to visit the restaurant.
- 126. There isn't enough parking in town so I don't visit. Build a parking garage then make all the parking spaces parklets.
- 127. Well, it is always nice to know there is outdoor dining at my favorite dining spaces
- 128. I knew there would be a place for me to safely enjoy my food outdoors
- 129. it seems attractive. I didn't know what to call them. Don't hang out downtown.
- 130. More people friendly / oriented
- 131. They make it far more convenient to enjoy downtown
- 132. It is a wonderful way to interact with the community! I am way more likely to eat outside and with summer coming I'll be even more eager to be downtown on the streets
- 133. More space
- 134. I'm much more likely to spend money downtown knowing these parklets are here.
- 135. being able to eat outside
- 136. Negative impact on parking availability
- 137. It is taking away from public parking to support private business. I have not complained due to city trying to find ways to support our business community through COVID. As we move out of COVID it is time to remove the park let's. I did not use them because I felt it encouraged people to gather and not social distance during COVID.
- 138. Used for restaurant service in COVID 19 crisis. Only good option is eat outdoors. I did not know that the parklets were not business related and could be used by anyone even if not using the business establishment. That would have been good to know
- 139. space to sit and enjoy!
- 140. Here's the problem from my perspective: traffic. Tra-freak! The noise, the squealing of breaks, the sheer number of vehicles all racing down Main Street. Absolutely essential to bring some traffic calming into play. I live at Burbank Heights and the traffic roars by us at the entrance/exit to our residences. If the development across Bodega goes forward traffic will be increased substantially. We used to have a police cruiser watching for speeders - that hasn't happened for at least two years. I inquired and was told they were waiting for a trainee to get through the traffic training. Focus on traffic calming, please!
- 141. Now that I know that I could use one, I'd be inclined to visit more often.
- 142. More enjoyable to eat and shop downtown with outdoors areas. Also it adds to the charm and quaintness of the town. Sometimes it feels like the town is drowning amidst the two highways crossing through. too much traffic and exhaust. The parklets help control that a little bit. I'd love MORE car-less areas and a REAL town square!
- 143. Made me feel that I could still enjoy downtown while being safe and outdoors. I love them!!!
- 144. It was great to know I could continue supporting the local businesses without spending time indoors, or just doing a grab and run, leaving downtown again right away.
- 145. I cringe to see the ones who are sucking up to the fumes.
- 146. Eating outside is safer
- 147. No impact
- 148. It has made walking and visiting downtown more relaxing. Felt like a touch of Spain.
- 149. More safe space to sit with my children
- 150. I like knowing they're there to help the businesses. I especially like the one at Screaming Mimi's. It should be permanent.

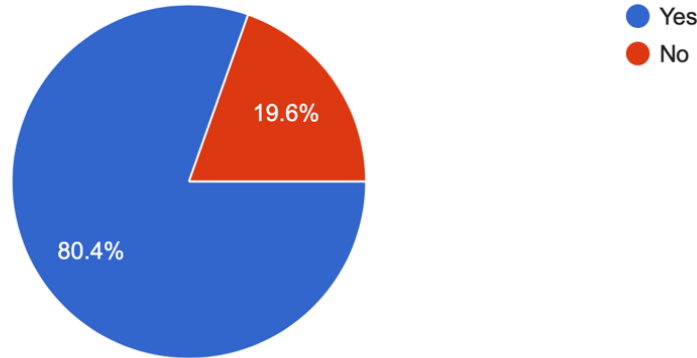


151. I feel so much safer outside than inside, these outside hosting areas have been amazing.
152. It impacts my visits to downtown because there are fewer places to park.
153. Especially during the pandemic but also before and after I prefer outdoor over indoor spaces to have coffee or a meal.
Having these spaces available has made me go get coffee many more times than before. The parklet in front of Screaming Mimi's is a no brainer. It made the corner safer and added a friendly welcoming touch to the entryway of our little town, which otherwise is nothing more than a
154. Made me want to walk around and stay longer
155. More visits
156. Love it!
157. I felt comfortable eating at a restaurant because of the parklet
158. I wouldn't be coming downtown as often if there wasn't a safe way to socially distance while enjoying my favorite snack places.
159. Outside seating!!
160. They just look inviting and it is nice to have a place outside to dine.
161. I came because of the parklet
162. I enjoyed eating outdoors. Then went shopping at local store.
163. We love the parklet options!!
164. Outdoor space to sit and eat
165. I'll take any opportunity to have ice cream. More space, more better!
166. I'm more likely to go to a downtown restaurant or coffee shop if I can eat/drink outside
167. It made me feel safer about taking my time to enjoy a meal or an ice cream, etc. but also it just made the downtown area feel more convivial, a lot like southern Europe.
168. The expand outdoor seating is important for my returning to restaurants.
169. limited parking
170. I love downtown already - this is an added bonus.
171. It calls attention to the store.
172. Like to eat outside because of Covid
173. Please keep them for health and safety and for the merchants!!
174. More inviting
175. I like them, make them permanent and more aesthetically pleasing.
176. Sebastopol is limited by the busy road through town - the parklets make the downtown faaaaarrrrr more attractive to me to shop and dine. Would love for them to continue!
177. It's more convenient to sit in a designated area vs. on some random benches, and I felt like it was safe, since the gables were spaced pretty far apart
178. There is already very little parking available downtown. Now with the parklets, I just assume that I won't be able to find parking and don't go downtown unless absolutely necessary.
179. I am more likely to frequent the downtown businesses knowing that there are outdoor seating options.
180. We frequent Screaming minis ice cream and it's nice for families sit and enjoy their ice cream right there.
181. Again happy for the food businesses and if there continues to be restrictions for indoor eating then they are needed.
When those restrictions are lifted we don't need to take up much needed parking spaces. If the parklets had more thought put into the overall look I would be more on board. Our town looks like a mess with K rails... really?
182. Even though I don't feel safe using them, because of COVID, I am fully in support of them
183. They add to the quality of the Sebastopol experience while deemphasizing the importance of cars and parking.
184. Quit getting rid of parking
185. I love having the additional spaces to sit and enjoy the local businesses. I know I've been more likely to come to downtown knowing I can sit and enjoy these spaces!
186. It allows me to attend local businesses, eat, and feel safe.
187. We have gone downtown more often knowing there will be places to sit/dine
188. Outdoor seating is great!



6) Would you like to see parklets continue in Downtown Sebastopol?

567 responses



6a) If you wish, use this space to tell us more about why or why not you would like to see parklets continue in the downtown area.

315 responses

1. Parking
2. Makes Sebastopol more homey and charming!
3. I think they're pointless.
4. I love the more European feel--a place to have a cappuccino and visit with friends as they walk by, or to have a meal with a friend while chatting and watching Life as it strolls by. ;-)
5. Yes, absolutely! The parklets encourage walking, community, supporting local businesses.
6. Test answer
7. See above
8. This makes our town more pedestrian friendly...also beyond the pandemic, it is nice to have the choice to sit outside
9. We need available parking more than eating areas.
10. Community feel, encourages more walking
11. I think they should stay also until the restrictions are lifted
12. Nice to be outside. They calm traffic. They encourage parking once and walking around which makes for more business at other downtown businesses.
13. I like having more spaces for community gathering
14. Crowds. Parking. Don't make Sebastopol into Healdsburg. Or, maybe that is you plan.
15. Sidewalks get crowded. The parklets are a great way to linger while socially distancing.
16. Once COVID is over we should go back to normal. The space is needed for traffic and parking cars.
17. makes the town feel cozier....gives more space to businesses....pulls people and and about in town...shows us more "locals" faces.
18. I think they should continue, but with a couple of caveats: 1) if the businesses on Main St support their continuation; and 2) if there is some way the town can beautify them. Those concrete barriers (even after being painted) are SO ugly. They don't look inviting at all.
19. More of a sense of community



20. builds community
21. Feels more inviting for there to be more outdoor gathering spaces in downtown.
22. I think the parklets really enliven the downtown area and provide more outdoor space for local businesses, which is helpful during the pandemic and beyond.
23. The park let's create a sense of community and places to gather in an urban environment
24. Nobody uses them. The cafes dont want them. All they do is look messy and take up too many parking which all the business in the area can utilize
25. I enjoy eating outside and they make our toen seem friendlier and more welcoming.
26. To make Sebastopol a more pedestrian friendly city, it's a great idea to keep the park lights and to increase the number of people enjoying our downtown spaces.
27. Increases livability for us downtown residents and creates a more welcoming town to visitors!
28. The parklets create a sense of community, and allow more socialization around the businesses in downtown.
29. Surely Some People Can Tolerate Eating In The Street
30. I see that people are supporting restaurants and eating at the park let's. I think they should continue until we reach the yellow level.
31. More seating is always good
32. The sidewalks on Main Street are too narrow for outdoor seating. We should either make the parklets permanent or reduce the width of the highway to make our sidewalks wider.
33. They are not attractive and they take up valuable parking spaces
34. People love people. I believe parklets will attract more otherwise thru traffic to slow down and join the fun. Patrons should be visible to passing traffic. Barriers must be substantial and creatively painted/attractive and clean.
35. Makes town more pedestrian friendly and vibrant.
36. They need to stay because they are helping businesses stay in business and a safer alternative. The places downtown are not very spacious indoors and I actually wouldn't recommend them if indoor seating g was the only option.
37. I have only used the screamin mimis one which has been nice, although somewhat noisy. I see others using the ones outside restaurants and if it means these businesses can survive I'm all for keeping them. As long as the council keeps the car park behind hop monk as a car park to make up for lost downtown parking.
38. Also the park let's take away valuable parking.
39. Waste of parking space
40. I love the European feel of it.
41. It's less parking for people who already don't have a lot of parking available to begin with.
42. Big benefit to downtown use.
43. Parking!
44. Less parking. Doesn't feel safe.
45. It helps business and customers during these unsure times with the spread of variants...
46. Good option for businesses. We need a vibrantly busy downtown.
47. We have the climate for outdoor activity, and these parklets are a great boon
48. The parklets are great for local business and pleasant for visitors.
49. Again - community space! We do not have enough open space or services like what the parklets provide in downtown Sebastopol. It has made such a great addition to downtown that guests and visitors alike have raved about. We look forward to continuing to use the parklets and their vital space.
50. We really need places outdoors! Small is good. It motivates me to walk to the downtown area. Let's have more!
51. yes, I like the outside experience
52. Parking has always been a huge issue. Now that we are back to pre-pandemic traffic patterns, we are returning to the same parking issues.
53. Waste of space.
54. I don't feel that business owners should be responsible to maintain the areas if the park let is not associated with their business. I like the idea of park let's but our downtown is not a great place for them. Too much traffic and our sidewalks



- are too narrow to easily accommodate them. If they were spread out around town where there is space and a way for the city to maintain them it might be nice to have a few.
55. I don't want it to hinder local businesses. there's space to equalize like to the right side of milk and Honey near the bank that's off the sidewalk.
 56. It's a nice space to get together with friends if you don't want to go to the Barlow, although I like the Barlow too
 57. Going forward we should keep the parklets to help create a more vibrant Main St/downtown with more places for people to be. Main st. sidewalks are too narrow so having more sitting areas is great.
 58. See above
 59. Because they are great
 60. I think that there are a lot of businesses downtown that don't have patio seating or good outdoor areas for their guests to use. In the past sidewalks/ability to walk through town has been impacted by spill over from places like Screamin Mimis who have been able to flourish even more with some outdoor seating.
 61. We need more outdoor space downtown. If they continue, they should be upgraded to quality materials and finishes.
 62. These are very friendly spaces and we Need a friendlier downtown feel.
 63. I'm ok with the ones on Main Street but not the one by Depot Street.
 64. I believe it supports community interaction
 65. I just wanted to address parking, which I know is a concern. I obviously never drive to the downtown area but have many friends who do and the parklets have made little to no impact on the accessibility of the downtown in terms of parking. All my friends park in the CVS lot or in the square. The parklets are just so wonderful and make me want to get out of the house and explore the town. I have shopped in more stores downtown than I ever did before. Seriously, they're so great!
 66. See answer for #5A
 67. I think they are a great idea - help the local businesses and contribute to vitality of downtown. Love outdoor cafes and dining and anything to encourage this.
 68. It creates community!!!
 69. More outside space and less space for cars is nice
 70. Makes downtown look more fun. You can see people you might know.
 71. Makes town feel more inviting - serving needs of all ages. Space for conversation, a respite from the sun. A bit European.
 72. I like sitting outdoors downtown to eat and socialize
 73. It creates more community building and connection. It's healthy to be outside more.
 74. They are disruptive to the flow of traffic.
 75. Our community needs to move towards being a greener city. I think seeing people walking, sitting at outside cafes, and biking leads to a healthier lifestyle.
 76. Impedance to traffic/parking
 77. They open up more space for people to congregate, slow down unsafe traffic, and make a more welcoming community feel downtown.
 78. Until there is no question that people are no longer getting infected with Covid, keep the parklets
 79. if they looked better and had plantings
 80. The restaurant's needs more support from you. You all did a piss poor job at it. Keep it. I think cement the area in permanently for the restaurants.
 81. The way people drive in this town, it's not very safe (on main st)
 82. They help me slow down while driving, they help the community feel more vibrant and user friendly
 83. False sense of security, the cement barriers, too much exhaust from the road, too close to traffic
 84. They make town feel and look vibrant
 85. they are fun...I like the idea that a particular business does not own it so it is free to all
 86. i say maybe to the parklets if they can be proved to be effective after the pandemic is over.
 87. Everyone benefits
 88. I think they have been a beautiful addition to downtown. They provide space for cyclists and families to enjoy their time while also being outside and distanced. They will be great post-covid as well to have more space to enjoy the goods people are receiving from the local shops!
 89. Yes, Keep the parklets! Outdoor space for people to gather that isn't the sidewalk is great fir the community



90. It gives people a safe place to sit and enjoy the outdoors
91. I don't think the question is fair as some may make sense if it is for a restaurant but others are just taking up the limited parking and detouring people from walking the block
92. Good for commerce = good for our town
93. More public space to be social, less car dominance everywhere. European feel.
94. Just makes it nicer to go down town.
95. I don't want them because they take up needed parking spaces.
96. It is just more ways for people to use and appreciate "public" space which is always in short supply
97. Have the public toilets opened . This is very important.
98. Makes downtown more fun
99. It makes downtown seem alive again seeing people enjoying themselves on the street.
100. I am concerned about parking. Maybe 3/4 park let's won't be enough to affect parking but it's nice to give business more space.
101. They are wonderful! Yes, yes I love the parklets.
102. being able to eat outside
103. They are in the way of parking.
104. No because it has taken away from public parking. Puts people closer to traffic and exposing them to more exhaust. Makes it harder to navigate the sidewalks do you larger gatherings. To name a few reasons.
105. They are very inviting.
106. They help create a lively outdoor culture which we should have living in the most beautiful place on earth :). They bring a European walking street feel and will draw visitors to our local business.
107. It's lovely to have an outdoor space to spend time, especially during these times. They create a great community atmosphere.
108. They never look like a cute relaxing spot to suffer in. I would never.
109. The parking of cars along the curb gets hectic for both pedestrians on the sidewalk and for drives through the main downtown section. It is a much smoother flow through that section without the constant hunting for parking and waiting for people to parallel park.
110. The park lets are friendly and welcoming. It's nice to see people enjoying themselves and helps the businesses
111. We need parking!
112. We have so many beautiful and warm days, in any other country of the world with comparable weather, outdoor seating would be amply available. Without parklets Sebastopol's outdoor spaces are basically reserved for cars (parking and streets with minimally sized sidewalks) and not people. Parklets add to the liveliness of the streetscape and invite people to walk instead of just driving through town. This is not my opinion, there are many studies that support these findings. The parklet in front of Screaming Mimi's is a no-brainer. It has made the corner safer and adds a friendly welcoming touch to the entryway of our little town, which otherwise is nothing more than a big crossing of two highways.
113. They're great, especially for summer!
114. Community!
115. Great for community
116. Makes the town feel vibrant and friendly
117. Leads to a sense of community. Great in the summer!
118. Nice visual and an inviting space
119. It just feels more human, convivial, like cafés and small restaurants in Europe. It invites us to slow down and enjoy things more. We walk by and see people we know and stop to talk, maybe even to stay and order something. It's just a much better art of living.
120. Long-term I'm concerned about the loss of parking. The limited parking in Sebastopol makes shopping difficult. I like the parklets and would support their staying permanently if there was additional parking.
121. They help improve the health of the community - both physically & economically.
122. The ones we have now are just fine.
123. Makes town seem more vital. Like in Europe.
124. It's nice to see people and not cars.
125. They make downtown attractive
And enticing to dine and shop.



126. Taking away parking spots harms our non-restaurant businesses and residents. It is unfair to take parking away from other downtown businesses in order to give seating areas to certain downtown restaurants.
127. Outdoor seating options make me feel safe and more likely to visit businesses.
128. They add to the quality of the Sebastopol experience while deemphasizing the importance of cars and parking.
129. Can't shop downtown, no parking
130. They are great, would like them to stay even after the pandemic. Much nicer than cars.
131. I think they are great for the downtown area
132. While I would never eat next to traffic, I support the businesses who will benefit from them.
133. F..k [redacted]Cars!
134. With vaccinations and re-opening of indoor seating, they will no longer be needed. They are not adequately safe from spreading Covid-19 before vaccinations are sufficient to re-open indoor seating. They are also an eye sore and obstruct traffic and parking. I support the general idea to help small businesses, but feel they were unsafe and not sufficiently effective.
135. I would like them to continue ONLY IF they are made more beautiful. Right now they look bad. Funky and hodgepodge. I've seen some well-done parklets on Clement St. in San Francisco and in Novato. These have attractive walls with planters built in. They are filled with flowers or ornamental grasses. They can be made of wood, rusty metal or some other material. I think the concrete barriers look terrible and if the only way we can have parklets, because of cost constraints or safety, is to keep these concrete hunks, I say the parklets have to go. The sad Charlie Brown trees and the concrete barriers in the Screamin' Mimi's parklet - ug! What an unappealing welcome to our downtown.
136. It is not safe for users - those blockades are not going to protect them from an errant car. Additionally it has a negative impact on parking and creates areas where the sidewalk is almost impassable because of servers crossing the sidewalk
137. Dangerous for patrons and traffic and pedestrians
138. The parklets make downtown more user friendly. I would love to see the parklet barriers painted with bright colors and scenes!
139. Those traveling through mention the inconvenience of going through Main St. Us residence want Main St to be more convenient... parklets are more convenient!
140. They bring energy and life to the downtown area
141. They definitely served their purpose during covid inside dining closures. As the pandemic recedes (please) they are no longer required. Future approval for each parklet should be approved by the citizens.
142. Support local restaurants.
143. Parklets add critical outdoor gathering spaces to our downtown
144. The ones on North Main Street do not seem to be used. The one on South Main is used by the coffee shop and restaurant, and the one in front of screamin mimi's is used, having those continue for a while would be fine.
145. Parklets provide extra space for our Main Street Restaurants
146. I would like for our town to have vibrant downtown street life and the parklets contribute to that.
147. Definitely would like to see them there. I go to SF frequently and always use the parklets.
148. Parklets make the downtown streets more usable and inviting.
149. I never go downtown.
150. We need more outdoor options for gathering. It is healthy and creates a visible sense of community and adds color and warmth to our rather bland downtown area.
151. They take up valuable parking spaces.
152. They are noisy, dirty, dangerous, ugly and take up parking. The only thing the serviced was outside eating during Covid.
153. Unsafe. Too much traffic.
154. Gives more space from cars
155. The spot in front of mimis is a great way to improve the appeal of downtown without sacrificing parking
156. Too much congestion in a tiny area
157. Why would anybody want to eat next to cars spewing exhaust?
158. They're an eye sore of construction barriers. They take up parking that's been argued is in short demand. There's empty spaces for rent, meanwhile we're giving up parking spaces for businesses to expand? It's free real estate for established operations (do I get a parklet for my new business?). Don't consider them safe, and doubt any has proper insurance coverage for these spaces (especially with them close to roads).
159. They are inviting and charming



160. It's OK to keep the parklets but I would not dine at their location because they are too close to vehicle exhaust on street. I have tried eating at an eatery several years ago that was close to sidewalk and the experience was unpleasant because of vehicle noise and exhaust.
161. traffic noise, congestion, I feel like I'm eating in a high pollution area
162. Nice to have the option to sit outside, especially a place like Mimi's
163. Sweet comfortable easy stopping spots to watch the world go by while enjoying goodies from local merchants or just hanging out.
164. Provides a safer environment for mask-free dining.
165. When I begin going out to populated areas again, I will choose places with outdoor space, so a parklet will draw me to downtown Sebastopol
166. Adds great energy to downtown with more dining options...has a European feel.
167. Helping Sebastopol restaurants serve more people safely is smart way to support businesses.
168. I feel these parklets enhance the community atmosphere of our downtown. I love the look of them, as well as the communal space they provide.
169. They are an eye sore n unsafe
170. It encourages customers who might have hesitations about indoor dining, especially families with small children who can't be vaccinated for COVID-19.
171. Only if they are done right, in the right place and not ugly
172. I think they are a sweet addition.
173. The Retrograde Parklet and Screaming Mimi's Parklet are well-used and seem to help local businesses. The one in front of Sumi and People's music is not used and takes up parking spaces. In general, the parklets are not visually attractive and in order for them to help downtown visually, they need to have better designs.
174. Contributes to the parking and traffic problem.
175. Seems good to still allow people to distance.
176. Looks more like community, helps the businesses, people can congregate safely, I would rather eat or visit outdoors than indoors, I say keep them or extend the time, some people may not be ready to enter inside places. What a wonderful idea!
177. They were a good idea while indoor dining was curtailed. No longer needed.
178. It gives more seating to the establishments that use them. It is nice to have an outdoor seating option for those restaurants that didn't otherwise have outdoor seating.
179. I'm in favor of anything that will decrease the presence of automobiles in downtown and that will facilitate both pedestrian traffic and businesses ability to increase their ability to service their clients. Downtown bars and restaurants deserve our help in these challenging times.
180. It's problematic for those who take their morning walks and don't want exposure so close to theirs because of Covid...my husband walks across street to avoid the park let's, but not a problem. So I am neutral about continuing
181. You need to open restaurants for indoor dining
182. I think anything that creates more pedestrian spaces to hang out downtown is good for downtown. I am immune compromised; otherwise I'd love to hang out in a parklet with a cup of tea! Someday I will when it is safe for me to do so. Plus it creates more outdoor space for downtown like there already is so much of at the Barlow.
183. The parklets need a floor that is level with the curb and that provides for drainage. One parklet has a plywood floor but these should be concrete public improvements.
184. Slows traffic, and less congestion as people do not attempt to parallel park in these areas.
185. Yes, if it helps the downtown businesses.
186. Yes, but other than the one by Mimi's and in the parking lot on the square, they seem unwelcoming due to proximity to traffic. Great idea, just need better placement.
187. I think the parklets support the local businesses provide more space for community and customers and I am for doing everything we can to support our businesses to thrive under any circumstances.
188. I enjoy a pedestrian lifestyle and it is nice to see others out, seated and relaxed, enjoying the day.
189. Great option for outdoor seating which we will need for many months to come, perhaps years.
190. Creates more community space.
Sidewalk is too narrow anyway.



- 191. I think the parklets add to the charm of the downtown area. Parking does not seem to be affected and people seem to be enjoying themselves. Additionally, I think the opinion of the downtown businesses should be considered when making this decision as they are the ones most impacted.
- 192. to support local businesses and to have more outdoor seating available
- 193. I'm a business owner and a resident - I'd like to see these parklets become permanent - get rid of the ugly cement blocks and ask business owners to create something permanent with protection (as San Francisco has done).
- 194. A better use of space than parking cars. A downside is heavy traffic on Main. Overall it makes main street more user friendly.
- 195. They provide more outdoor area for patrons...feels more European which to me is a plus!
- 196. Although I have not used the parkletes, I love seeing people sitting out like that together and I also feel more COVID safe in our town because folks can be outside/downtown/together in a safe way, which makes us all safer. And I do hope it encourages more commerce downtown, again good for all of us!
- 197. I think the parklets are a great opportunity for additional public space as downtown sidewalks are so narrow. This allows a place to pull off to the side to chat with someone you may run into, or for downtown businesses to have outdoor seating. I think they are a great benefit and do not miss the parking spaces
- 198. Gives a community feeling in spite of the traffic
- 199. The parklets add a European cafe style feel to our town, which I like.
- 200. Creates more community and calms traffic.
- 201. The parklets add value by creating more places for people to gather.
More people = more business.
Please make the parklet by Mimi's a permanent street closure and add furnishings.
- 202. Besides the issue that it is unappealing to sit right out in traffic, these spaces serve very few customers, and as the need for them will soon be greatly or entirely diminished, any benefit from them will soon be far outweighed by the negative factors of the disruption to traffic and parking.
- 203. I think they encourage restaurant patronage and also give a European feel to our lovely city.
- 204. No longer needed.
- 205. Not enough public parking downtown. The privately owned lots are for costumers not for going to other stores.
- 206. Sebastopol has some of the best weather in the world. Let's take advantage of it by eating or hanging out in the fresh air.
Look how many people use the outdoor areas at the Barlow, any modern retail design space will have plenty of outdoor space and downtown businesses should be able to offer the same to their customers.
- 207. Love seeing people outside, sitting around, enjoying themselves. Makes downtown come alive.
- 208. Create more feeling of community
- 209. customers love to eat outdoors it appears that we are open and a vibrant community
- 210. It gives Sebastopol a friendlier demeanor, less cars - more people, and our weather is so conducive to using outdoor spaces
- 211. Creates community spaces
- 212. I like the feeling of walking or driving in town and seeing people outdoors eating. When I was in Italy, this was very common and added to the happy ambience. My two concerns are: less parking for customers and exhaust from cars going by. If I could have it my way, I would propose pedestrian only blocks. I will say, however, that using parking spaces for tables does not really bother me that much because we are talking about a commercial district, and the trade off is worth it. As long as there is parking available within a reasonable distance, I am all for it. However, what I don't like is restaurants in residential areas that have turned their designated parking lots into outdoor dining rooms. This negatively impacts the neighborhood where people live on streets that are now taking the brunt of restaurant parking that is no longer available.
- 213. Impedes traffic flow and parking.
- 214. The more places available for people versus cars, the better!
- 215. It seems like it would attract more patrons of the downtown businesses. I don't have disposable income so it has not attracted me.
- 216. Friendlier, livelier environment to have people out and about, not just cars!
- 217. during time that indoor seating is limited
- 218. This pandemic is not over no matter what the CDC says about vaccinated people. We need to continue providing safe places for people to shop in Sebastopol.



219. Tourist towns can be confusing. This sudden squeeze down of traffic can lead to accidents. Google maps does not see parklets. Parking in short supply. Bike lanes are impacted as well. High school population on rise more new drivers. Thank you for asking.
220. I like seeing people out having fun when I am out and about. Very European. I did not know they were public - may use them more now.
221. It feels safer to be outside. And I feel it gives us all more sense of community
222. They add to the business chance of success and are a great outdoor dining option.
223. It will be nice to have a choice between inside or outside dining.
224. might mitigate traffic.???
225. Not attractive with cement barriers on Main Street. The tent in the Plaza is in the way of neighboring businesses. This is all perfectly fine while indoor dining is limited, but once things return to normal I'd like to see them removed.
226. I'd love to see more outside dining!
227. Although I have not used them yet, they look very appealing; I am old and tire easily, walking downtown, and knowing that they are there is very helpful.
228. Outside seating is still best, weather permitting.
229. It is a nice option if you want to sit and have coffee and visit with a friend or actually if it's adjacent to a restaurant to be able to have some thing to eat outside in the fresh air.
230. I have a parklet directly in front of my studio. I own Create It! While it may be a good place for people to eat ice cream and food, I've had only two customers agree to use it to paint outside.

I am strongly encouraging the parklet to be gone by this October when it starts to be cold and rainy. It is blocking off the street in front of my business where customers drop off and pick up their kids from our after school program.

The only time we have use the parklet was to have an Easter Egg hunt out in it for our after school art program in April with our students.

231. Not sure --I have never used them - and probably would not. Whatever is decided - it is important to recognize that older folks cannot walk very far so parking that provides easy access to businesses is important.
232. People need outdoor spaces to patronize local businesses and safely socialize.
233. I think having outdoor seating (especially for eating) is an important healthy option during covid
234. It feels more like a close knit community to see people enjoying the outdoors while eating and visiting. Love it.
235. If it helps people feel safe then yes.
236. It's a great way to make downtown more people-friendly.
237. The temporary parklets are a footing for permanent parklets as far as the community and even Caltrans is concerned. A new wave of permanent parklets should be designed and implemented safely. There are grants for complete streets programs which should be aggressively pursued.
238. I think just an accident waiting to happen, eating in pollution with so many cars driving by, and not attractive
239. Nice option for safe social distancing while covid variants are still emerging.
240. Nice change of pace.
241. I think they make the downtown area more of a community feel, more of a European flavor, friendly and communal.
242. Parklets provide a retail/open space opportunity, helps retail, allows Covid - cautious patrons to still frequent restaurants without eating inside.
243. Are you referring to the one at Mimi's and in front of the coffee shop on Main Street? I don't know of any others.
244. The outdoor space for people is welcome. It adds to a sense of community. Visually, the parklets are more attractive than parked cars.
245. I think it only positively impacts the town. Missing a few parking spaces doesn't impact use.
246. But I prefer to not see them in the street.
247. I have been supporting our downtown economy by running my restaurant for the last 10 years and there has not been any improvements to Main street except new traffic lights and ADA curbs. Our downtown corridor needs a major facelift and the concrete surrounds only add to the ugliness of main street. People think things are under construction and we are losing parking. If we could design nice parklets like every other town in Sonoma County has that provide attractive and vital extensions of business space it would be another story. Businesses do not want to clean up other peoples mess and be responsible if the parklets are just open space with no purpose.
248. friendly and communal



249. Great outdoor seating
250. They give a friendly look to the downtown area. Even though I have not used one yet, I frequently walk downtown. They give a "slowing down" and festive feeling to Main Street, even to pedestrians.
251. One in front of Sunshine and Screaming Mimi's seem well-used. The other one - People's Music - should be removed.
252. I can't choose both answers above, so here is my response: Yes, I think they are good for the local shops and it's nice to have places to sit outside to rest or gather your thoughts for a moment. -AND - No, The concrete barriers look pretty tacky and they reduce the already limited parking.
253. Main street parklets are of marginal benefit due to proximity to traffic. Screaming Mimi's is an ideal location but it needs to be much better looking with plants and nice looking barriers.
254. I want our businesses to thrive, this may increase foot traffic downtown.
255. It is a friendly addition to downtown and increases foot traffic.
256. The one in front of Mimi's is nice, or the concept is. It looks a bit trashy with the concrete blocks and "Charlie Brown Christmas" trees. If it was more permanent I'd prefer to be looking at a more stylish and beautified structure and atmosphere.
257. To support local restaurants
258. They make our downtown more inviting and social.
259. In general, less car traffic would be better. We're not bringing back the train anytime soon, but assuming that parking can be had around the downtown area, I think parklets are a good way to increase usable pedestrian-friendly spaces in an otherwise excessively car-oriented stretch of road in the city.
260. It attracts business to the area. I love sitting outside on a sunny day or a warm evening, and now have more options. And it makes the business look more noticeable and festive. In Europe all the cafes have outdoor seating. It brings a quaintness to downtown and makes a business that was less noticeable have a chance to increase their business!!! It makes people want to visit downtown area again... The Barlow is booming because it has that atmosphere. People like it. It is helping our more depressed Main St. and plaza area revive and start to thrive again!!!! We need to build on that to stimulate our small business economy! Let's beautify our downtown even more.
261. I think they are awesome! It brings life to our very dead town. I would think seeing happy people eating would bring more customers as well. Then they will go shopping in the awesome shops downtown too.
262. Unnecessary and obstruct traffic. Dangerous right next to hwy12. Dangerous for pedestrians in parklet because there are not adequate barriers or barricade to prevent traffic and vehicle from slamming into people in joying parklet. Will end up being a law suit and legal issue for the city when a parklet pedestrian gets slammed by a car.
263. I love seeing all the people outside - feels vibrant
264. Now that we are vaccinated we would like to use the parklettes. We will not dine indoors for some time yet.
265. I like the idea, similar to spaces I have seen in SF. It seems they are being used here, especially in good weather. I would like to see them on side streets, in quieter areas, as well.
266. Main Street and S. Main
Are used by customers.
267. Parking needed for stores. Too much traffic so not pleasant.
268. It's a nice place to gather with others and maybe share some food and conversation
269. Outdoor dining is a great thing when weather permits (spring, summer, fall)
270. I'm old and not very mobile and prefer available parking to shop Downtown
271. I have so many customers who really enjoy the parklet. Although I would like to do more to improve the area, I am very happy with the parklet.
272. More car free hang out space is a plus.
273. They create a feeling of inclusivity & feel very European to me. I love the open expansiveness.
274. The parklets add some character and much needed space for socializing during these wild times. I have especially loved the parklet in front of screamin mimi's and retrograde.
275. No discernible impact. Convenient. Friendly.
276. Pedestrians > cars
277. They are an eyesore
278. It helps local business owners, particularly restaurants. And it enhances the feeling of downtown to have people eating outdoors.
279. It's an option for folks who aren't comfortable dining indoors
280. Let's put pedestrians before automobiles for a change.



- 281.It's nice to have a place to sit and eat ice cream.
- 282.They are such a good idea. It's space where people can see each other, talk to one another. I love the community aspect of them.
- 283.I have seen no obstruction to traffic flow, and impact on parking seems negligible.
- 284.Impeding traffic and parking and both of those items were already a problem to begin with.
- 285.Parklets are a fantastic tool for converting auto-centric spaces to a public pedestrian place making useful spaces for people. They bring people together in a public setting, helping promote business and encourage the use of pedestrian space otherwise designated for vehicles. Statistics reveal that parklets also encourage and promote better sales and add value to the small business owner. Parklets are a win for both environmental and economical reasons.
- 286.Gathering places are essential for community-building.
- 287.They look like barricades in a war zone. Some creative landscaping & better design is needed .
- 288.-Lack of parking
- Parklets do not seem to increase social distancing safety
 - Distraction to drivers/ risk to parklet patrons
 - parklets will soon not be necessary due to restrictions lifting
- 289.Safety hazard. Unsightly.
- 290.I like how they add to the downtown... I like seeing people out and about.
- 291.Feeling of community and will use them now that I'm vaccinated. Feels safer than indoor dining.
- 292.Parklets enable a few businesses to survive,
- 293.Having a park let to meet at, has made it easier to meet up with a friend.
- 294.I really like the idea of more outside seating.
- 295.Having the parakeets give us a nice dining option... outdoor dining.
- 296.Running two highways through a confusing for tourist area is ripe for trouble from a collision. It is not fare
- 297.With restrictions loosening and set to be removed almost completely by mid June our parking is a valuable resource that needs to be restored. Furthermore parklets that block entire through streets such as in front of Screaming Mimi's serve to aggravate an already poorly designed traffic flow situation in downtown Sebastopol.
- 298.Parking is never a problem and additional out door dining is desirable.
- 299.The weather here is great and for the businesses the ability to use both inside and outside will increase their recovery and add to the city's coffers I hope.
- 300.They are nice in certain areas where they are out of the way (Mimi's ice cream) but other areas (116 downtown)seem obtrusive (and who wants to eat dinner on the highway after COVID?)
- 301.Parking has been a challenge in Sebastopol for decades I've lived here since 1957. This only further reduces parking spaces. I often bypass Sebastopol and go to SR to shop because I can't find convenient parking.
- 302.It would be nice to have them more uniform, looking more permanent and professional/welcoming. In addition, assuring they look like a resting/nibbling/ hangout spot for anyone. I assumes they were "reserved" for adjacent restaurants.
- 303.Messes with traffic. Why would anyone want to eat next to a car spewing exhaust?
- 304.I think they allow more space for people to gather in town. I hope the city council approves keeping the parklets.
- 305.Reduces parking reduces safety was helpful but not any more
- 306.I do not think they are safe. Too many distracted drivers.
- 307.cars should not dominate public spaces. i am glad that we have claimed more space for pedestrians.
- 308.I don't like eating in the road. They also remind me of being locked out of life.
- 309.I love that it encourages more people to spend time outside! (And as a special education professional I love that having the dining areas off the sidewalk makes downtown more ADA accessible!)
- 310.They're a good way to make our streets seem less like highways
- 311.More cafe culture is great! And post covid- I'll always prefer to sit outside.
- 312.Parklets make our downtown more inviting and help support our struggling downtown.
- 313.They make the town feel more welcoming and provide more places for people to gather and enjoy the day and the town, especially during busy touristy months.
- 314.I would like to see the parklets continue permanently because they makes Senastopol more accessible for walking and gives the community opportunities to gather in small groups, gives places for people with young children and seniors a place to rest- . It just makes sense for a more walkable community. However, I feel the city could go a long way in making them aesthetically more pleasing, like living screens and large pots of plants, etc. This would give a buffer and soften traffic noise, and they could be lit with string lights or other low light at night. Suggestions from other communities who



do this nicely- Santa Cruz for instance. I think our community could only improve with this integration. They should also include more bike racks- there are NONE on Main St. The concrete barriers I. Front of Mimi's are very ugly! And that is at the entrance to our town!

315. Needed to increase vibrancy downtown

Please use this space for other thoughts, questions, or suggestions about parklets in Downtown Sebastopol.

169 responses

1. Whatever it takes to support the businesses
2. We need Sebastopol to be more of a pedestrian city. Fix all the sidewalks between Luckys & Firrest on both Main, Petaluma & healdsburg Ave. also fix all of Bodega between the bridge at the Laguna & Ragel
3. Pls consider keeping the parklets.
4. I am so glad that parklets are being tried out. I think they could benefit from additional design and detailing, including plants or other simple structures, and be more attractive.
5. Having the trees that have been placed around the park let's and some of the parklands is really sweet to like creating a little plant zone to create another sense of invitation to sit down and relax and eat and drink at the establishment downtown
6. Maybe some decorative lighting.
Please find something (planters?) other than the UGLY concrete blocks to protect patrons.
7. The parklets are great, and we'd love to keep them!
8. How Can A City Of 7,700 People Afford Your Pension ?
9. I think the screamin mimis one should be made permanent as the little access road made that corner dangerous for children outside the pottery shop and ice cream parlor.
10. add more! They bring people out.
11. People violate 2 hour parking on Bodega nearly every day knowing that they most-likely won't get ticketed. They say they HAVE to because of the parklets.
12. I know Caltrans is hard to deal with but please consider making them permanent
13. I like that the barriers are painted with bright colors. Maybe coordinate the colors somehow...or not. Art elevates every space.
14. I would make them larger so that there is less confusion about whether the lane is available for cars or not.
15. Parklets in Downtown Sebastopol should be encouraged and expanded as much as possible.
16. Parklets are a good idea lets keep them if we can..
17. Thanks for asking for community engagement. It sure is the best way to create policy
18. The argument that parking is affected has not been the case.
19. My only concern is that sometimes they get a bit crowded. I would like to see a bit more space between households, especially during this transitional period when many are not vaccinated and people are less inclined to wear masks outdoors.
20. I didn't know there were even talks of taking away the parklets around town to be honest; I had assumed that the city would put more effort into making them even better or permanent. I've seen them utilized in downtown areas of other cities so well and they can be amazing beautification projects for downtown spaces as well when people add things like street art or plants. If the city does decide to take away parklets I'll be disappointed -- we have a large community of at-risk people and I thought that everyone would do their part to keep as many amenities from the COVID era as possible to keep everyone safe and comfortable instead of going back to maintain some flawed status quo from before all of this.
21. Love them! Keep them!
22. I believe our sidewalks are dirty and need refurbishing. But the park lets soften that issue.
23. It's great that Sebastopol has tried to help businesses with this idea.
24. KEEP THE PARKLETS !!



25. Thank you!
26. More please
27. A dream would be to divert cars out of the downtown and have only people and biking. It's our future.
28. Please keep it going and make more parklet spaces available! It's the next step in making SEB more walkable, bikeable and enjoyable.
29. where is the submit button on this form? Oh, MUST click on something in question 4 to get the submit button. That is a lame design. Why didn't someone test this before deploying it?
30. I think the city has failed the business. Other cities have built really nice ones and each restaurant had there own spot. I did not vote for any of you. In fact I want to vote all of you out.
31. There is already NO parking in SEBASTOPOL, AND THESE PARKLETS TAKE UP MORE OF THE PARKING THAT WE DID HAVE
32. There should be more!! One at the Library would be the BEST!
33. There is No parking in Sebastopol as it is. These take away from the parking and it is as if your sitting in a lane of traffic, exhaust filled reckless drivers. No Thank You
34. Please have basic design standards to ensure that aesthetics and quality of materials are considered
35. thanks for asking!
36. see previous answer
37. Please don't get rid of them!
38. If we would have a parking garage (where the proposed hotel is supposed to go is best suited for this) parking on the street could be replaced by parklets with only a few spaces designated for deliveries. This would also facilitate movement of traffic through town.
39. To me, parklets are cute and kind of a funny way but earnest way of addressing the lack of open public space. Except for the Square, sidewalks, and the green parks, there is little public space for citizens to roam and congregate and find union, unlike Europe and other countries.
I don't know what the answer is, other than knowing that kind of grand public spaces in America are not our future.
40. SF has lots and lots of parklets with a much more impacted parking situation. If they can do it, so can we.

I want even more parklets.

41. Thank you for asking for community input
42. I live outside of Sebastopol so need to drive in. I believe we need more city owned free parking near downtown, especially as more people venture out.
43. Please - let's keep the parklets and create a path to having more of them!
44. Parking is more important than sloppy (temp) street eateries. And cars a zooming by, or idling right next to you, as you eat?????
45. There is, and has always been, a group of people in Sebastopol, who freak out when any parking space is turned over to actual human usage. I am tired of their screams. It's time to make people more important than cars.
46. Love ❤️ them! Good decorations also!
47. I think Sebastopol has much to gain by creating more of a village atmosphere. It's heathier, inclusive and will promote better relations between people.
48. Please keep the parklets at least through the summer months.
49. I would like to see more!
50. Please stop the parklets. It seems immoral to help a few restaurants while ignoring the other businesses of Sebastopol.
51. Outdoor seating options make me feel safe and more likely to visit businesses.
52. Parking is a huge issue
53. I really like the parklets in the Plaza and hope they become permanent.
54. Be sure that use of parklets is as required by Caltrans policy. Available to the public. Not free real estate to select businesses.
Also, charge for all parking.
55. With vaccinations and re-opening of indoor seating, they will no longer be needed. They are not adequately safe from spreading Covid-19 before vaccinations are sufficient to re-open indoor seating. They are also an eye sore and obstruct



traffic and parking. I support the general idea to help small businesses, but feel they were unsafe and not sufficiently effective.

56. Some towns have collectively maintained flowering pots or hanging baskets that make such a difference in creating an appealing, welcoming downtown. If we want a downtown that survives into the increasingly online world, we have to be willing to invest in this kind of improvement. The increased sales tax would help with the costs. I'm in favor of adding more large, attractive planted pots and baskets to Main St. in general.
Even though the noise and fumes from the traffic being so close is a big downside, I could imagine beautiful parklets improving Main St. They can also have a trellis with little lights. Ornamental grasses can be a good screen, are low maintenance and don't require much water. However, creating attractive parklets would be a considerable expense, especially the irrigation. If the parklets can't be beautiful, I think they should be removed.
57. Take them out.
58. Let's add more wherever we can
59. Please keep them open
60. The need for them seems to be dissipating. We have ample green space in the plaza. Probably best to remove them at the end of the year or when restaurants are fully open.
61. I hope we are able to continue to have our Parklets through summer months
62. I think we should have as many as possible!
63. My concern is parking, and has been for years. Doing any significant shopping followed by a meal is nearly impossible until after 6. People bypass shopping and eating downtown due to limited parking, One hour -3 hr parking is limiting. And a business owner I have toovey car multiple times in a day. Please address parking options which allow us to make up the lost parking spots releases for seating areas.
64. As long as they don't affect traffic then they are fine.
65. The Sebastopol downtown area is a mess already with traffic and people. No park let's please.
66. I guess someone is going to have to be gravely injured before the people in this town wake up to the stupidity of this.
67. Some are not terribly nice looking or well done. If they continue, I hope business owners will put aesthetic energy into design.
68. Please be sensitive to the non-dining businesses that have lost parking spaces in favor of the parklets.
69. Please keep them, at least through the fall. Our community is just beginning to venture out with a greater sense of freedom, and it will help tourists feel more comfortable this summer.
70. I'd like them to look better, with better paint or even designs, plants, decorative lights.
71. Remove the one in front of Sumi Body and People's music. Give money to build nice, visually pleasing parklets so the current ones do no look so haphazardly designed and constructed. You could make downtown look a lot better by removing unused newspaper boxes, planting, weeding meridians and window cleaning.
72. I'd let them stay, if I were to eat out I would only do so at a place with outdoor dining.
73. The weather getting nicer, outdoors is better right now for gathering of people. 🙌👍
74. We need the parking.
75. As a former downtown Main Street business owner this had been discussed before and I am glad to now see them downtown. I hope they stay, I think it is added ambiance and would like to see them even improve on the design if they are allowed to remain.
76. In fact rather than "parklets". the parking lane should be removed and the sidewalk widened for two whole blocks of Main Street.
77. Recommend making designated parking spaces clearer or only on one side of the street, to stop bottlenecking. I would not do both parklets and 10 minute pick up in the same area, as that has felt chaotic.
78. The only issue is parking will be limited. Creating these parklets sometimes take up valuable parking spaces. As traffic is increasing we need more solutions for easy parking.

Also hope these parklets do not invite after hours loitering. Our homeless issue from what I hear from Barlow merchants is getting worse. I'm not sure what the Sebastopol police department is doing about it. I hope there is a plan.

79. I appreciate this opportunity to express my ideas/thoughts.
80. Make more of them, restrict vehicle traffic and make downtown Sebastopol more pedestrian friendly.
81. Stop the Benedetti truck wash



- 82. I'd like to keep the parklets until the local businesses are steady and recovered from the financial impacts of the pandemic
- 83. Probably will never happen, but I'd turn 3 downtown blocks into carless peds only space.
- 84. Thank you!
- 85. My only wish is that the parklets were better and more creatively designed. Even painted, the concrete barriers are not particularly attractive. I personally believe they are not necessary. It is extremely unlikely that a car would run into a parklet. There are so many examples of beautifully designed parklets. I think better designed parklets would encourage more use. I hope we can create nicer permanent parklets to replace the current versions.
- 86. The parklet at Screaming Mimi's does make my traffic flow a little more inconvenient. Personally, I don't care about that parklet as I don't sit and eat ice cream there, but if the general community prefers to retain it, I'd be fine with the slight inconvenience.
- 87. There should be even more parklets on Main St., and other downtown streets too. Although, I do feel the existing parklets could still use some additional enhancement with plants, trees and overhead structures.
- 88. I wonder if these parklets pose any actual safety concerns. If the businesses find these parklets beneficial for their bottom line, that would be a factor to weigh in favor of retaining them, but otherwise there is no reason at all to retain them.
- 89. A few more parklets would be great. Some trees and plantings in them would be great too. And build some safe and separate bike trails too while improving the town.
- 90. Keep them.
- 91. Plaza is under utilized. Transition to this space as a common area and do not advantage one business over another with a parklet right outside one business but exclude a competitor. We at HairMasters have private parking why can't we have dining in parking lot (parklet)? No one approached us, Would appreciate some feedback. Steve the Property Manager.
- 92. Parking on Healdsburg Ave is pretty bad since caltrans took the ability way after the reconstruction of 116. And at times parking is pretty tough to find.
- 93. If they become permanent they need a better design. Other towns have done much better with this (Healdsburg Plaza for example).
- 94. I think it's fine while the weather is still nice. However I'd like to see the Parker gone on Sebastopol Ave/Depot st in front of Create It and Screamin Mimi's ice cream when the weather gets cold in the fall.

It prohibits cars from being able to pull up in front of my shop Create It and unload kids safely onto the sidewalk.

- 95. Keep the parklet's.
- 96. While only a few businesses were able to directly benefit - Sunshine Cafe, happily and perhaps People's Music with respect to visibility of the store - a more inclusive program could be established to embrace other businesses in other districts. While the immediate need for parklets may seem to be passing, these elements if well-designed could enhance the overall character of the town and stimulate a walking community.
- 97. I wish we had even more areas with outdoor sidewalk seating. Like in Paris. So friendly and inviting. As a local I have great resistance to the Barlow. It's expensive and feels very pretentious. Opening the downtown Main St feels much more inviting and authentic.
- 98. The parklets should be better designed and more permanent rather than just K-barriers. See San Francisco's design guidelines. Great job getting Caltrans do do something out of the ordinary. That's not easy.
- 99. I would rather see a pedestrian street than these contrived spaces encircled by industrial concrete highway barriers.
- 100. I want to continue to eat outside
- 101. We have an abundance of park space downtown and never enough parking. Can parklet space/tables be designated in the green areas near the gazebo or the Peace Wall?
- 102. I am a Business owner and I am embarrassed by our Downtown with its dirty sidewalks awnings and garbage cans. We have old dying trees that make a mess and no thought has gone into updating our approach to Main Street. The one way is also a continuing nightmare. Lets GET REAL and stop spending money on questionnaires, consultants and surveys and spend our city funds on much needed actual improvements.
- 103. could be made more attractive
- 104. Keep them! We need more
- 105. I hope they continue!
Sorry there can't be one at Gypsy Cafe.



106. Spend more money making downtown tidy. A lot of weeds in tree wells. Public parking lots don't look too nice. There are missing bricks around the library facade. The parklets should be nicer looking - they look like concrete walls. Maybe add more plants and wood features to them so they are more natural.
107. Sadly, if there is no accessible route to the public parklet area (curb) then the disabled folks will complain and possibly sue the City for equal access. If they are located a curb ramps for wheelchair access to the street level then they are probably okay.
108. Parking needs to be made available off of Main St. and clear signs used around town to point out parking areas.
109. Whatever is in front of People's Music looks janky, trashy and unused. It seems like it is more disruptive to the flow of (bicycle) traffic.
110. Expand the use of parklets
111. I think we need more parklets and quaint atmosphere and beautification brought to downtown. It feels like it would bring way more business to our part of town that people just drive pass. The Barlow is fun. Main St. needs that... Downtown San Rafael has white lights strung zig zagging down the main street, 4th St., that looks romantic and enchanting. It attracts people and makes you want to be there. On Thursday and Friday night they block off 4th St. and all the restaurants set up tables out on the street and people gather and eat so they could survive thru the pandemic. It has remained because of all the benefits and it's festive and it's allowing things to flourish. We need that here. Bring back our quaintness to Main St.... Please. It will make the city money as well. Thank you for asking.
112. Please allow more!
113. Good to do it during pandemic to help restaurants. OK to do it when indoor dining is unsafe.
114. More sun umbrellas. The sun is too strong. We need shade to enjoy ourselves.
115. I like them!
116. Does the City of Sebastopol wish to be recognized as a auto (vehicle) centric town with plenty of parking or a small town with places for people to walk, gather and enjoy in a casual setting. To promote ample sidewalks and green streetscapes for people to gather as a community is the future. Environmental health suggests that residents and business benefit from a green ribbon populated by people and green spaces, not that of gray fields lined with polluting vehicles.
117. This would be a positive result of a Covid development. There will be others...
118. Please consider keeping them. It gives our town more of a pedestrian feel... A pedestrian positive atmosphere
119. Parklets are a creative solution to a temporary problem.
120. Keep them!
121. The barriers around the parklets should be a bit more attractive. The old concrete barriers could be replaced with something else or painted. Maybe concrete planters could be used?
122. For other merchants to block parking supporting one business rather than another.
123. I would like to see more outdoor eating venues in Sebastopol but not on the main roads.
124. I think allowing business close to the parklets use the space for their patrons is important. I see how well they work in San Francisco. It brings more people out of their homes. They are good for community building.
125. Let's keep them!
126. Create more of them, have local artists and designers decorate them, prioritize community and gathering over cars passing through.
127. Please look at how other cities have developed these in their downtowns.
128. Even though I haven't used them, I would like to after the pandemic when it's more safe for my family.
129. It looks like businesses aren't using them anymore?
130. Is not safe.
The city already screwed up painting new lines on the road through town a few years ago. The drunk guy driving the painting vehicle couldn't keep it straight for more than a block at a time. Every block an offset starting point. Then having to waste more money to have another guy grind it off and attempt to fix stupid. Well you can't fix stupid.
131. Please help businesses make the park let spaces more attractive and complete 🙏
132. Like to see more of them like the Barlow
133. The parklets could be made much nicer with a small amount of funds. I have pictures of the parklets in Fairfax that are much more friendly and architectural
134. Making these contiguous with restaurant frontage would provide a better experience and invigorate downtown.
135. Can the parklets be more like the ones in San Francisco? They should be made of wood or other attractive materials, not just concrete barriers.
136. Thanks for keeping this wonderful idea alive!



- 137. More parklets and more pedestrian space downtown... all good ideas. Downtown would be best if it were no through traffic and pedestrian friendly.
- 138. I think this would be something for limited use. Parking downtown and traffic downtown is already terrible. If it was permanent it should be done well and advertised that it is for everyone regardless of business. Local restaurants should be encouraged to take advantage such as delivery. It should be shielded from traffic as well as possible. No taking up the entire block.
 It might be better to take some of the parking near Hop Munk on Main Street for the purpose.
 It would also be helpful to get Cal Trans to synchronize the traffic lights downtown East-West and North-South so that Cars aren't sitting there spewing exhaust fumes
 While I am at it, someone is an idiot that developed the bike lanes. I am a cyclist and do not need a whole traffic lane/superhighway for my bicycle which begins at one point in the middle of town and ends in the middle of town, slowing auto traffic. If there is to be parklets, use them as a reason to increase the traffic from south to north on Petaluma Ave. PARKLETS MAY MAKE THIS WORSE! TRAFFIC NEEDS TO SPEED UP IF PARKLETS ARE KEPT!
 Thank you
- 139. Of course, if the amount of traffic could be scaled back - I know, unlikely - or at least subject to some calming.
- 140. Close Main Street for cars and make Sebastopol friendlier for walking with children.
- 141. Thanks to the city and businesses who helped make these happen!
- 142. The old gas station lot adjacent the Basso building could be a great little park space.
- 143. Figure out how to make more parking!!!!
- 144. They're okay, I guess.
- 145. Too much traffic to enjoy outdoor dining. Reduce to one lane.
- 146. Great job getting them in! Diana Rich you are doing such a great job! Thank you!
- 147. I've seen how lively Valencia Street in SF has become with the parklets and it helps to close a block or two. This would be a big pull to get people downtown.
- 148. There dangerous please remove
- 149. Why would the people of Sebastopol not check out the child sex offender closely before they made him mayor of Sebastopol
- 150. I liked the wooden ones you tried a couple years ago. The feel was nice but maybe the concrete is necessary for safety.
- 151. Improve our sidewalk situation.
 Put a sign at the crosswalks with flashing lights to tell the cross walkers to activate the lights when there is a SAFE SPACE to cross. Don't just hit the button and step out into the street. There are several times when people have hit the button and stepped right on into the street without looking for cars coming at all!
 Get rid of the crosswalk at Petaluma and Depot Street. They can cross at the intersection. That one is unsafe! If you have a green light going north on Petaluma, cars behind you are not expecting you to stop or slow down for pedestrians shortly after you cross the intersection.
 Do something about the four-way crossing at North Main in Burnett Street. That has two lanes of traffic going in one direction and no flashing lights or indicators when there are four crosswalks at that intersection. It is very dangerous.
- 152. I enjoy the outdoor seating on the sidewalks, but not a fan of the spaces that take up valuable road space.
- 153. please add more parklets. The downtown is too dominated by 116 and the cars.
- 154. Make them prettier ~ perhaps with shade coverings.
- 155. Parklets should stay
- 156. I think any decision to keep them should have an end date with a requirement to make an affirmative decision to continue in the future.
- 157. The bike lanes have already disrupted traffic tremendously. I do not wish for more traffic or delays
- 158. Sebastopol's traffic will get worse with the merging of high schools. Parklets are taking up parking spaces
- 159. It's time for them to go.
- 160. Best to block through traffic on main street. Time to re-reroute
- 161. A traffic free downtown would be a great incentive for me to visit small businesses and restaurants in Sebastopol!
- 162. Not sure if they should all continue. The one in front of screaming mimi's seems best because traffic is not as close due to the meridians. But the ones with just the barricade between sitting and traffic don't seem so good and I would not use them unless there was an absolute need.
- 163. More areas to sit are great.
- 164. I support creating pedestrian-friendly urban spaces that encourage community activity and patronage at local businesses



165. I love Sebastopol ❤️

166. Limited parking is already an issue in Sebastopol. Taking away parking spots only makes this problem worse.

167. I'd like to see parking and support services for the homeless. Drinking water, more bathrooms, parking, and a shower if possible.

168. Reduces parking.

Distraction while driving.

Will not be needed soon due to restrictions lifting.

169. Too many 10 minutes loading spots



Kenneth S. Jacobs

Attorney at Law
117 North Main Street
Sebastopol, CA 95472

(707) 829-7303

Email: KEN@KENJACOBS.COM Website: KENJACOBS.COM

Certified Specialist

Estate Planning, Trust & Probate Law
State Bar of California Board of Legal Specialization

February 3, 2022

City Council
City of Sebastopol
7120 Bodega Avenue
Sebastopol, CA 95472

RE: February 15, 2022, City Council Meeting – Parklets

Dear Mayor Slayter and Council Members,

I am an owner of the Town Plaza Professional Building, located at 107-121 N. Main, adjacent to the Ramen Gaijin restaurant. I would like to offer the following comments regarding the “parklet” on the west side of the Town Plaza, which consists of an open white tent structure.

I fully supported the tent “parklet” in order to assist my neighbor, Ramen Gaijin, during the pandemic lockdown. At that time indoor dining was prohibited, and I am pleased the tent “parklet” assisted the restaurant with maintaining a viable business. I also want to acknowledge and thank Ramen Gaijin for willingly moving the tent out of the way last spring when we needed to access the area for a crane to lift seven new HVAC units onto the roof of our building.

That said, at this point the tent “parklet” does not seem necessary. I have never seen any use of the tent other than for the benefit of Ramen Gaijin. The restaurant is now fully open, including their indoor dining area and the outdoor patio. The tent structure appears to be rarely used, other than as a storage space for a few tables and chairs. It is unattractive, messy, blocks the view of neighboring businesses, and could potentially impede maintenance and emergency vehicles.

I have no objection to the restaurant using that area in the evening for outdoor tables. However, it seems at this point, with the restaurant fully open, that can be done without the tent.

Thank you.

A handwritten signature in blue ink, appearing to read 'Ken Jacobs', written over a light blue horizontal line.

Ken Jacobs

Kari Svanstrom

From: Kathleen Escamilla <[REDACTED]>
Sent: Thursday, February 03, 2022 12:59 PM
To: Kari Svanstrom
Subject: Re: Parklet discussion item at City Council Feb 15

Hi Kari

We own Portico at 110 N Main. We are nearest to the parklet in front of music store. We would like to see it gone for a few reasons. It's not kept clean and attractive, it's not well used at all, and it's taking precious parking spots.

Thank you,
Kathleen

On Feb 2, 2022, at 7:53 PM, Kari Svanstrom <ksvanstrom@cityofsebastopol.org> wrote:

Hello all,

I hope this email finds you well. I'm emailing you because you have previously been involved with meetings, surveys, or are a nearby business owner to one of the parklets in the Sebastopol Downtown. The City Council will have an agenda item regarding parklets, and your input would be appreciated.

Also, if you would like to submit a public comment for inclusion in the staff report (we've received communications previously, but do not assume folks want those included as public comments), please send to me by next Tuesday for inclusion in the staff report. Comments received after that will still go out to the City Council, but may not be included in the published agenda report.

The agenda report and meeting info (zoom) will be published at the below link by the Thursday before the meeting:

<https://www.ci.sebastopol.ca.us/Meeting-Event/City-Council/2022/City-Council-Meeting-February-15,-2022>

Thank you all

Kari Svanstrom, AICP, Architect
Planning Director

City of Sebastopol | Planning Department
7120 Bodega Avenue | Sebastopol, CA 95472
(707) 823-6167 phone
www.cityofsebastopol.org



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